

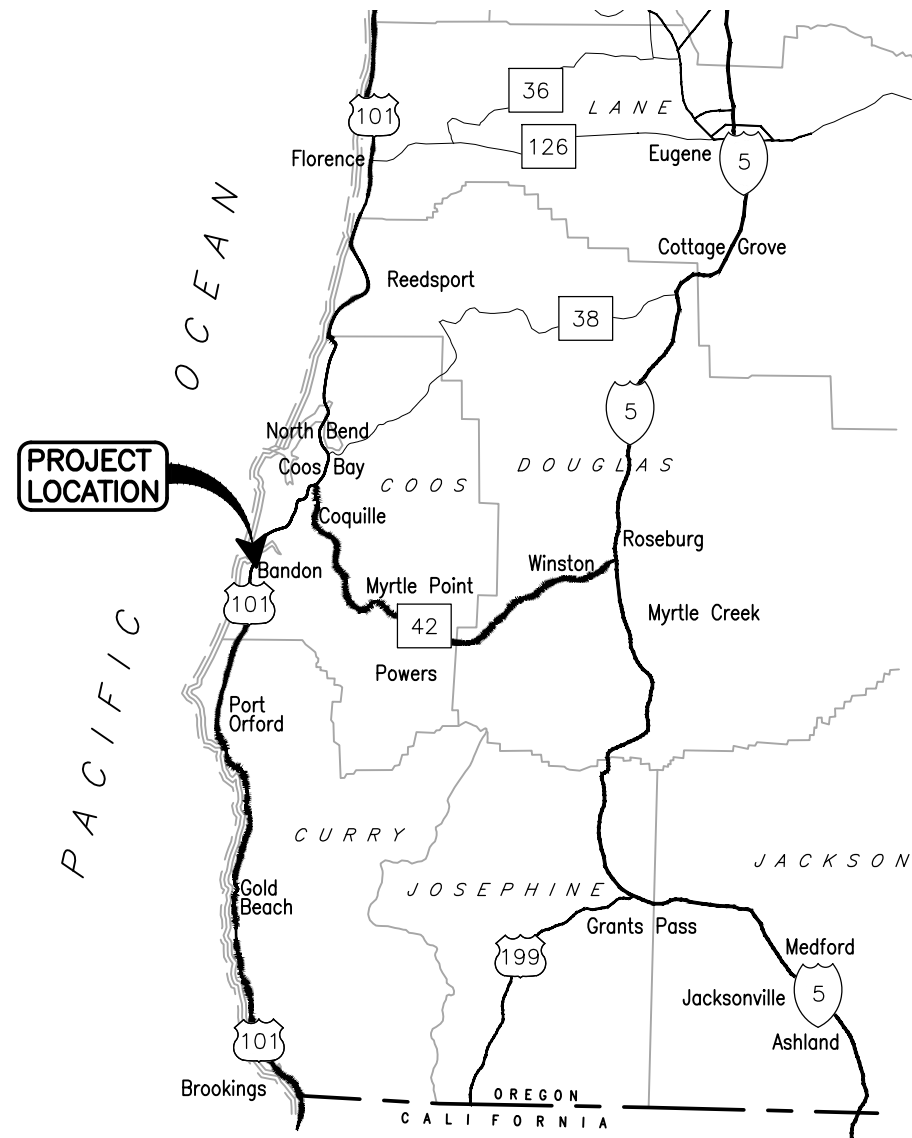
PORT OF BANDON MARKET PARKING IMPROVEMENT PLAN

BANDON, OREGON

PREPARED BY:

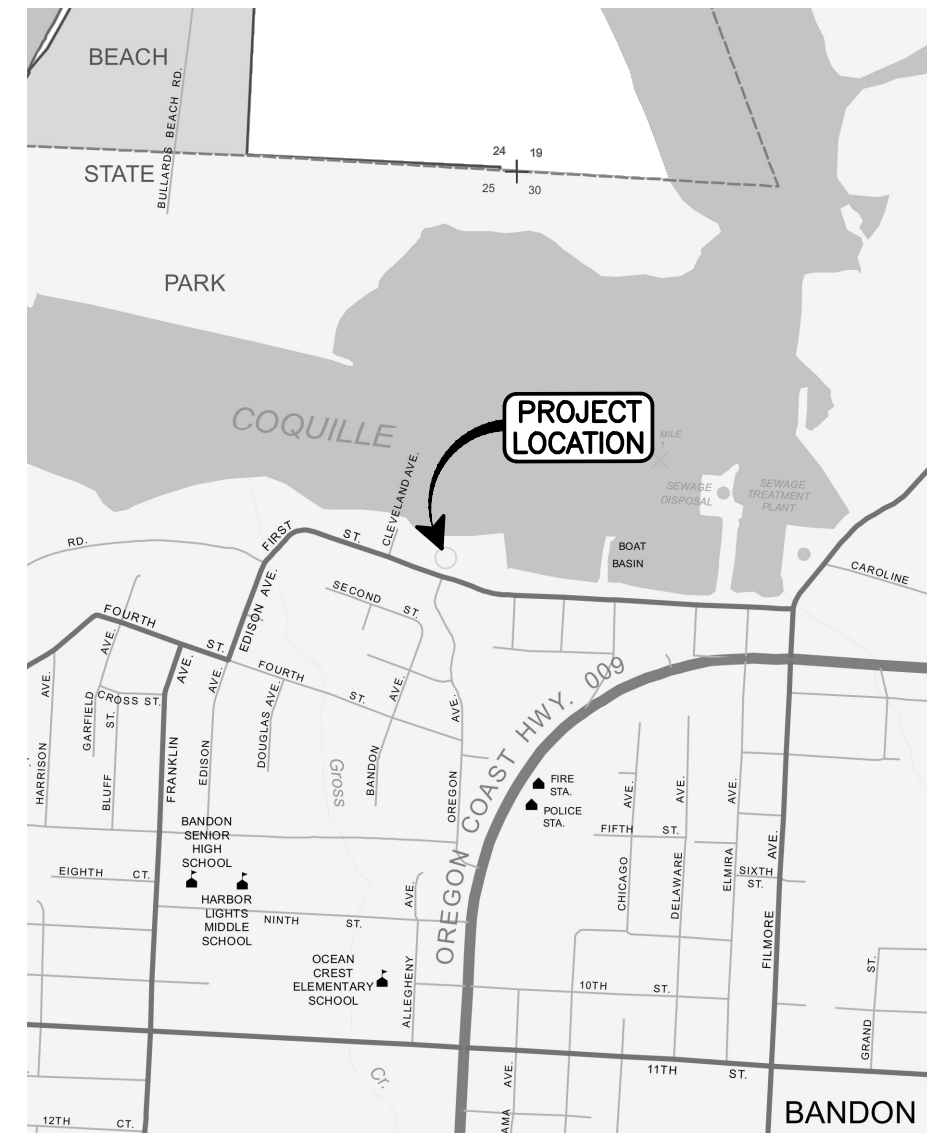


APRIL 2021



LOCATION MAP

NTS



VICINITY MAP

NTS

SEQ	SHT	TITLE
G-1		COVER
G-2		STANDARD ABBREVIATIONS AND LEGENDS
G-3		GENERAL NOTES
ESC-1		DEMOLITION AND ESC PLAN
ESC-2		ESC DETAILS
C-1		SITE PLAN
C-2		GRADING PLAN
C-3		GRADING PLAN DETAILS
C-4		GRADING PLAN DETAILS
C-5		GRADING SECTIONS
D-1		DRAINAGE DETAILS
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D-4		CIVIL DETAILS

SHEET INDEX



Oregon law requires you to follow rules adopted by the Oregon Utility Notification Center. Those rules are set forth in OAR 952-001-0010 through OAR 952-001-0090. You may obtain copies of the rules by calling the center (503) 232-1987.

All underground utilities and substructures shown hereon were obtained from the best available sources, and are presumed to be accurate, and complete. It shall be the contractor's sole responsibility to verify, locate, and protect all utilities and substructures shown or not shown.

CALL THE STATEWIDE UTILITIES LOCATING SERVICE AT:
(800) 332-2344 W/YOUR SCHEDULE AT LEAST
TWO DAYS PRIOR TO BEGINNING WORK.



VERIFY SCALES
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275 MARKET AVENUE
COOS BAY, OR 97420
WWW.SN-OR.COM
541-266-9880



NO.	DATE	REVISION	BY

APVD	RFS	DSR	DPG

PORT OF BANDON
MARKET PARKING IMPROVEMENT PLAN
BANDON, OREGON
COVER

SHEET
G-1
SEQ
DATE
4/2021
PROJ. NO.
618074

CONSTRUCTION NOTES:

GENERAL

- CONTRACTOR TO COORDINATE ALL WORK WITH THE PORT OF BANDON AND EXISTING BUILDING OCCUPANTS.
- ALL WORK SHALL CONFORM TO CURRENT OREGON BUILDING CODE, CITY OF BANDON MUNICIPAL CODE, OR OREGON STANDARD SPECIFICATIONS FOR CONSTRUCTION AS APPLICABLE.
- ALL WORK WITHIN THE RIGHT-OF-WAY SHALL COMPLY WITH THE OREGON TRAFFIC CONTROL HANDBOOK LATEST EDITION.
- ALL LOCATIONS FOR WORK SHALL BE CHECKED AND COORDINATED WITH EXISTING CONDITIONS IN THE FIELD BEFORE BEGINNING CONSTRUCTION. EXISTING UNDERGROUND UTILITIES WITHIN THE LIMITS OF EXCAVATION SHALL BE VERIFIED AS TO CONDITION, SIZE AND LOCATION BY UNCOVERING, PROVIDED SUCH IS PERMITTED BY LOCAL PUBLIC AUTHORITIES WITH JURISDICTION, BEFORE BEGINNING CONSTRUCTION. CONTRACTOR SHALL NOTIFY ENGINEER OF ANY DISCREPANCIES.
- THE CONTRACTOR SHALL SECURE ALL NECESSARY PERMITS PRIOR TO THE COMMENCEMENT OF CONSTRUCTION.
- THE CONTRACTOR SHALL PROVIDE TRENCH SHORING FOR ANY TRENCH OVER FIVE FEET IN DEPTH IN ACCORDANCE WITH OSHA STANDARDS.
- CONTRACTOR AGREES THAT IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, GENERAL CONTRACTOR WILL ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THE PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY.
- ALL WORK AND EQUIPMENT SHALL COMPLY WITH OSHA SAFETY REQUIREMENTS. THIS REQUIREMENT SHALL BE MADE TO APPLY CONTINUOUSLY, AND NOT BE LIMITED TO NORMAL WORKING HOURS. CONTRACTOR FURTHER AGREES TO HOLD HARMLESS, INDEMNIFY AND DEFEND THE OWNER, THE ENGINEER AND HIS/HER CONSULTANTS.
- THE CONTRACTOR SHALL INDEPENDENTLY REVIEW GROUND, TOPOGRAPHY, AND CONDITIONS THROUGHOUT THE SITE, AND ASSUME THE RISK OF COMPLETING THE WORK SET OUT ON THESE PLANS, REGARDLESS OF ROCK, WATER TABLE OR OTHER CONDITIONS WHICH MAY BE ENCOUNTERED IN THE COURSE OF THE WORK.
- ANY DISCREPANCY DISCOVERED BY THE CONTRACTOR IN THESE PLANS, OR ANY FIELD CONDITIONS DISCOVERED BY THE CONTRACTOR THAT MAY DELAY OR OBSTRUCT THE PROPER COMPLETION OF THE WORK SHOWN HEREIN SHALL BE BROUGHT TO THE ATTENTION OF THE OWNER AND THE ENGINEER IMMEDIATELY UPON DISCOVERY. SAID NOTIFICATION SHALL BE IN WRITING.
- ALL UNDERGROUND IMPROVEMENTS SHALL BE INSTALLED TESTED AND APPROVED PRIOR TO PAVING.
- GRADING AND CONSTRUCTION CONTRACTORS SHALL STOP WORK AND NOTIFY THE OWNER AND THE ENGINEER IF CULTURAL RESOURCES ARE DISCOVERED DURING CONSTRUCTION.
- THE CONTRACTOR SHALL GIVE THE INSPECTOR 48 HOURS ADVANCE NOTICE OF ANY CONSTRUCTION OR REQUIRED TESTING.
- SHOULD THE CONTRACTOR OR ANY OF HIS AGENTS OR EMPLOYEES ENCOUNTER OR DISCOVER MATERIALS WHICH APPEAR TO BE HAZARDOUS DURING THE PERFORMANCE OF THE WORK, THE CONTRACTOR SHALL INFORM THE ENGINEER IMMEDIATELY AND SUSPEND WORK IN THE AFFECTED AREA UNTIL THE ENGINEER HAS INSPECTED THE LOCATION AND MATERIALS IN QUESTION. SHOULD IT BE NECESSARY TO UNDERTAKE REMEDIATION, THE ENGINEER WILL GIVE WRITTEN NOTICE TO SUSPEND WORK IN THE AFFECTED AREA UNTIL THE PROPER COURSE OF ACTION HAS BEEN DETERMINED. OPERATIONS IN THE AFFECTED AREA SHALL BE RESUMED ONLY UPON WRITTEN NOTICE BY THE ENGINEER.
- ALL SITE GRADING WILL BE INSPECTED BY THE ENGINEER. COMPACTION TESTING WILL BE CONDUCTED AFTER SUFFICIENT DENSITIES HAVE BEEN ACHIEVED IN THE CONTRACTOR'S OPINION. THE CONTRACTOR SHALL MAKE ALL REQUESTS FOR MATERIALS TESTING AT LEAST 48 HOURS IN ADVANCE. ANY SOILS THAT FAIL TO MEET THE REQUIRED COMPACTION LEVELS SHALL BE REMOVED, AND RECOMPACTED. ALL COSTS ASSOCIATED WITH ACHIEVING COMPACTION STANDARDS SHALL BE INCLUDED IN THE CONTRACTOR'S ORIGINAL BID.
- THE TOPSOIL SHALL BE REMOVED FROM CUT AND FILL AREAS AND SHALL NOT BE USED FOR ENGINEERED FILL. TOPSOIL SHALL BE STOCKPILED SEPARATELY AND UTILIZED IN LANDSCAPED AREAS.
- NO CHANGES OR MODIFICATIONS SHALL BE MADE TO THESE PLANS WITHOUT WRITTEN APPROVAL BY THE ENGINEER.
- THE WORKING DRAWINGS ARE GENERALLY DIAGRAMMATIC. THEY DO NOT SHOW EVERY OFFSET, BEND OR ELBOW REQUIRED FOR INSTALLATION IN THE SPACE PROVIDED. THEY DO NOT SHOW EVERY DIMENSION, COMPONENT PIECE, SECTION, JOINT OR FITTING REQUIRED TO COMPLETE THE PROJECT.

GRANULAR MATERIALS:

- FURNISH THE FOLLOWING MATERIALS FOR FILL OR BACKFILL WHERE SHOWN OR REQUIRED:
 - CLASS A BACKFILL: USE SELECT NATIVE OR COMMON MATERIAL THAT, IN THE OPINION OF THE ENGINEER, MEETS THE CHARACTERISTICS REQUIRED FOR THE SPECIFIC SURFACE LOADING OR OTHER CRITERIA OF THE BACKFILL ZONE. NATIVE MATERIAL SHALL BE FREE FROM ORGANIC OR OTHER DELTERIOUS MATERIAL, AND FREE FROM ROCK LARGER THAN 3 INCHES. IF STOCKPILED MATERIAL BECOMES SATURATED OR UNSUITABLE, CLASS B, C OR D BACKFILL SHALL BE USED.
 - CLASS B BASEROCK OR BACKFILL: 3/4-INCH DENSE-GRADED AGGREGATE MEETING ODOT SECTION 02630.10.
 - CLASS C BACKFILL: USE CLEAN SAND WITH NO PARTICLE SIZE LARGER THAN 1/4-INCH.
 - CLASS D BACKFILL: USE PIT RUN OR BAR RUN MATERIAL, WELL GRADED FROM COARSE TO FINE. THE MAXIMUM DIMENSION SHALL BE 3-INCHES.
 - CLASS E BACKFILL: USE CONTROLLED LOW-STRENGTH MATERIAL (CLSM) CONFORMING TO ODOT SECTION 00442. SLURRY SHALL CONSIST OF A HIGHLY FLOWABLE LEAN CONCRETE MIX; MIXTURE OF PORTLAND CEMENT, FLY ASH, FINE AGGREGATES, WATER AND ADMIXTURES AS REQUIRED FOR A MIXTURE THAT RESULTS IN A HARDENED, DENSE, NON-SETTLING, HAND EXCAVATABLE FILL.

SURVEY NOTES:

- BENCHMARK FOR THIS SURVEY IS LOCATED IN CENTERLINE OF 1ST STREET WEST SIDE OF OREGON AVE.
1/2" IRON ROD WITH RED PLASTIC CAP ELEV = 13.88
- VERTICAL DATUM IS NAVD88.
- CONTOUR INTERVAL IS 1.0 FOOT.

EROSION CONTROL:

- SEE SHEETS ESC-1 AND ESC-2.

DEMOLITION:

- REMOVE EXISTING ASPHALT, GRAVEL AND SUBGRADE AS REQUIRED TO PROVIDE THE NEW PAVEMENT SECTION TO THE LINES AND GRADES SHOWN ON THE PLANS. CONTRACTOR TO EXERCISE CAUTION TO PROTECT UNLOCATED UTILITIES IF PRESENT. FILL ALL VOIDS WITH STRUCTURAL FILL.
- MATERIALS GENERATED DURING DEMOLITION SHOULD BE TRANSPORTED OFF SITE OR STOCKPILED IN AREAS DESIGNATED BY THE OWNERS REPRESENTATIVE.
- IF MATERIALS CONTAINING ASBESTOS OR OTHER HAZARDOUS MATERIALS ARE ENCOUNTERED ON THIS PROJECT, CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR PROPER IDENTIFICATION OF MATERIALS, SAFE HANDLING, AND PROPER DISPOSAL. ALL APPLICABLE FEDERAL, STATE AND LOCAL RULES, LAWS AND GUIDELINES MUST BE STRICTLY ADHERED TO OVER THE COURSE OF CONSTRUCTION.
- CONTRACTOR SHALL COORDINATE PAYMENT FOR PERMITS, FEES, INSPECTIONS, ETC. WITH APPROPRIATE REGULATORY AGENCIES. CONTRACTOR TO PROVIDE SAFE TRANSPORT TO DISPOSAL FACILITIES.

EARTHWORK:

- THE CONTRACTOR SHALL BE RESPONSIBLE TO CONFIRM EXISTING GROUND ELEVATIONS, OVERALL TOPOGRAPHY, AND GENERAL SITE CONDITIONS PRIOR TO THE START OF CONSTRUCTION.
- COMPACTION REQUIREMENTS AS SPECIFIED WILL BE BY PERCENT OF THE MAXIMUM DRY DENSITY (MDD) AND AS DETERMINED PER ASTM D 1557/D6938.
- PLACE FILL MATERIALS, BASEROCK AND TRENCH BACKFILL MATERIAL IN LOOSE LIFTS OF NOT MORE THAN 8 INCHES FOR MATERIAL COMPACTED BY HEAVY EQUIPMENT, AND NOT MORE THAN 4 INCHES FOR MATERIAL COMPACTED BY HAND-OPERATED TAMPERS.
- THE GROUND SURFACE IN AREAS TO RECEIVE FILL SHALL BE PREPARED AS FOLLOWS:
 - ALL ORGANIC MATERIAL AND TOPSOIL SHALL BE REMOVED.
 - ON SLOPES GREATER THAN 1V:4H, HORIZONTAL BENCHES SHALL BE CUT INTO THE SOIL TO PROVIDE A LEVEL-BEARING SURFACE FOR THE FILL MATERIAL. THE MINIMUM WIDTH OF THE BENCHES SHALL BE FOUR FEET.
 - COMPACT SUBGRADE TO 95% MDD.
- ALL IMPROVEMENTS SHALL BE GRADED TO DRAIN TO THE APPROVED DRAINAGE COURSE AT A UNIFORM SLOPE OF 2% MINIMUM UNLESS OTHERWISE NOTED.
- NO CUT OR FILL SLOPES SHALL EXCEED THE SLOPE RATIO OF 2H:1V, UNLESS OTHERWISE NOTED.
- TOPSOIL SHALL BE REMOVED FROM ALL CUT AND FILL AREAS AND SHALL NOT BE USED FOR ENGINEERED FILL.
- FILL MATERIALS SHALL BE CLASS B BASEROCK OR OTHER UNIFORMLY GRADED GRANULAR MATERIAL APPROVED BY THE ENGINEER.
- FILL MATERIALS SHALL BE MECHANICALLY COMPACTED. JETTING WILL NOT BE ALLOWED.
- CARE SHALL BE TAKEN NOT TO CRUSH THE PIPE OR OTHER COMPONENTS WITH COMPACTION EQUIPMENT.

TESTING AND INSPECTION:

- FINAL SITE GRADING, SHALL BE INSPECTED BY THE ENGINEER. CONTRACTOR TO PROVIDE 48 HOUR NOTICE IN ADVANCE OF REQUIRED INSPECTION. FAILURE TO GIVE ADEQUATE NOTICE MAY RESULT IN TESTING DELAYS WHICH WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- THE CONTRACTOR SHALL FACILITATE ALL TESTING AS REQUESTED BY THE ENGINEER. THE ENGINEER WILL BE PERMITTED TO REQUEST AND RECEIVE TESTING RESULTS AT ANY FILL OR BACKFILL LOCATION OR ELEVATION DURING THE CONSTRUCTION PROCESS.
- THE CONTRACTOR SHALL SCHEDULE TESTING BY THIRD PARTIES WHEN IN HIS OPINION SUFFICIENT DENSITIES HAVE BEEN ACHIEVED.
- TRENCHES SHALL BE TESTED EVERY 50- FEET AND FOR EACH THREE (3) FEET OF LIFT. TESTS SHALL BE AT RANDOM LOCATIONS, OR LOCATIONS SELECTED BY THE ENGINEER, AND WITH A MINIMUM OF TWO TESTS PER ANY LENGTH OF TRENCH.
- A FAILING COMPACTION TEST INDICATES THAT THE REQUIRED COMPACTION STANDARDS HAVE NOT BEEN ACHIEVED. ANY FILL MATERIAL OR PORTION OF FILL MATERIAL THAT DOES NOT MEET THE SPECIFIED REQUIREMENTS SHALL BE RECOMPACTED UNTIL THE REQUIREMENTS ARE SATISFIED, OR REMOVED AND REPLACED AT NO ADDITIONAL COST TO THE OWNER.
- ALL COSTS ASSOCIATED WITH ACHIEVING COMPACTION STANDARDS INCLUDING RE-TESTING SHALL BE BORNE BY THE CONTRACTOR.

PAVING:

- ASPHALT CONCRETE SHALL BE 1/2" DENSE, LEVEL 2 ACP AND SHALL CONFORM TO SECTION 00744 OF THE OREGON STANDARDS AND SPECIFICATIONS FOR CONSTRUCTION 2015 OR THE LATEST EDITION.
- TACK COAT TO CONFORM TO ASTM D-977 FOR "EMULSIFIED ASPHALT," RVS-1 OR RS-2 GRADE OR APPROVED EQUAL.
- ASPHALT CONCRETE MATERIAL SHALL BE PLACED TO THE MINIMUM THICKNESS REQUIRED ON THE PLANS.
- WHERE NEW PAVING MEETS EXISTING PAVEMENT, EXISTING PAVEMENT SHALL BE SAWCUT, PLACE NEW PAVEMENT AND BASE TO SECTION SHOWN ON THE PLANS OR MATCH EXISTING THICKNESS WHICHEVER IS GREATER.
- APPLY TACK COAT TO CONTACT SURFACES OF CURBS, GUTTERS AND EXISTING PAVEMENT.
- COMPACT PAVEMENT BY ROLLING TO A MINIMUM OF 90% OF MAXIMUM DENSITY. DO NOT DISPLACE OR EXTRUDE PAVEMENT FROM POSITION. HAND COMPACT IN AREAS INACCESSIBLE TO MECHANICAL ROLLING EQUIPMENT. PERFORM ROLLING WITH CONSECUTIVE PASSES TO ACHIEVE SMOOTH FINISH WITHOUT ROLLER MARKS.
- AGGREGATE BASE MATERIAL SHALL BE CLASS B BASEROCK AS DEFINED ABOVE AND COMPACTED TO 95% MDD PER ASTM D1557/D6938.
- COMPACT SUBGRADE TO 95% MDD PER ASTM D1557/D6938.

SITE WORK CONCRETE:

- ALL CONCRETE CONSTRUCTION SHALL CONFORM WITH THE MINIMUM STANDARDS SET FORTH IN OREGON STANDARD SPECIFICATION SECTION 00440 COMMERCIAL GRADE CONCRETE LATEST EDITION.
- MIX DESIGNS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PRIOR TO CONCRETE PLACEMENT.
- UNLESS OTHERWISE STATED, CONCRETE SHALL MEET THE FOLLOWING CRITERIA:
 - CURBS, SIDEWALKS, THRUST BLOCKS, MISCELLANEOUS STRUCTURES, MINIMUM 28-DAY COMPRESSIVE STRENGTH = 3,000 PSI
 - RETAINING WALLS, MINIMUM 28-DAY COMPRESSIVE STRENGTH = 4,500 PSI
 - MAXIMUM AGGREGATE SIZE = 3/4"
 - SLUMP = 4"±1"
- REINFORCING SHALL BE DEFORMED BAR, INTERMEDIATE GRADE STEEL CONFORMING TO THE REQUIREMENTS OF ASTM A 615, GRADE 60, PLACED IN ACCORDANCE WITH THE CONCRETE REINFORCING STEEL INSTITUTE (CRSI) "MANUAL OF STANDARD PRACTICE."
- SURFACE OF ALL CONCRETE FLATWORK SHALL BE IN ACCORDANCE WITH OREGON BUILDING CODE (OBC) REQUIREMENTS FOR ACCESSIBLE ROUTES.
- FINISH CURB AND SIDEWALK SURFACES WITH LIGHT BROOM FINISH.
- ALL ITEMS TO BE CAST IN CONCRETE SUCH AS REINFORCING DOWELS, BOLTS, ANCHORS, PIPES AND SLEEVES SHALL BE SECURELY POSITIONED IN FORMS BEFORE PLACEMENT OF CONCRETE.
- WALKWAYS SHALL MEET THE ACCESSIBILITY REQUIREMENTS PROVIDED IN THE OBC. LONGITUDINAL SLOPES OF WALKWAYS SHALL NOT EXCEED 8.3%, MAXIMUM DISTANCE BETWEEN LANDINGS IS 30- FEET. CROSS SLOPES OF WALKWAYS SHALL NOT EXCEED 2%. LANDINGS SHALL NOT EXCEED 2% SLOPE IN ANY DIRECTION, AND SHALL BE 4- FEET BY 4- FEET MINIMUM.

STORM DRAIN:

- THE CONTRACTOR SHALL INSTALL ALL STORM DRAIN IMPROVEMENTS TO THE LINES AND GRADES SHOWN ON THE PLANS.
- ALL MATERIALS SHALL BE FROM NEW STOCK AND DELIVERED IN NEW CONDITIONS.
- ALL STORM DRAIN PIPE AND FITTINGS SHALL BE FACTORY MANUFACTURED AND CONFORM TO ASTM D3034 WITH GASKETED JOINTS. PROVIDE C900 GASKETED JOINT PIPE AND FITTINGS WHERE LESS THAN 36-INCHES OF COVER IS PROVIDED.
- PIPE ENDS SHALL BE CAREFULLY CLEANED BEFORE PIPE IS JOINED. INTERIOR OF PIPE SHALL BE KEPT FREE OF DIRT AND DEBRIS.
- INSTALL NO. 12 COPPER WIRE ALONG TOP OF PIPE. WIRE SHALL EXTEND TO SURFACE AT ALL MANHOLES AND CLEANOUTS, AND SHALL TIE TO TRACER WIRE ALONG EXISTING STORM DRAIN SEWER PIPING.
- INSTALL PIPE ACCORDING TO THE TRENCH DETAIL. TESTING SHALL BE IN ACCORDANCE WITH THE UNIFORM BUILDING CODE.

LANDSCAPING:

- RESTORE ALL DISTURBED AREAS TO SMOOTH LINES AND GRADES.
- COVER ALL DISTURBED AREAS WITH MULCH AND LANDSCAPE SEED OR COVER AS SHOWN.

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 541-266-9880



BY	REVISION	DATE	NO.

DSSN DPG
 DR DSR
 CHK RFS
 APVD

PORT OF BANDON
 MARKET PARKING IMPROVEMENT PLAN
 BANDON, OREGON

SHEET
G-3
 SEQ
 DATE 4/2021
 PROJ. NO.
 618074



GENERAL NOTES

SAVED: 4/20/2021 5:03 PM DGRABER, PLOTTED: 4/20/2021 5:13 PM GRABER, DAN
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REMOVE ABANDONED VAULTS AND BACFILL WITH COMPACTED BASEROCK

DOOR TO FISH MARKET

ABANDONED VAULTS
 CUT AND CAP EXISTING 6" DRAIN PIPE 1' FROM BUILDING WITH AN APPROVED MECHANICAL OR GLUED CAP SYSTEM

BAY DOORS ENTRANCE TO FARMERS MARKET

ABANDON PIPE IN PLACE, PLUG OR CAP PORTALS ON EACH END. REMOVE EXISTING CATCH BASIN

DOOR TO FARMERS MARKET

DEMOLISH AND REMOVE CONCRETE SLAB SIDEWALK, CURB ETCETERA

PRESERVE AND PROTECT EXISTING ELECTRICAL VAULT

DEMOLISH EXISTING CATCH BASIN

SAW CUT & DEMOLISH EXISTING ASPHALT, CURB & GUTTER

(E) CATCH BASIN
 GRATE= 14.75
 6" PVC INV OUT=13.71
 6" PVC INV IN(NW)=CAPPED

(E) CATCH BASIN
 GRATE= 13.02 N 12.95 S
 15"(ID HDPE) INV OUT(NE)= 10.96
 8" PVC INV(SE)= 10.98
 8" PVC INV(SW)= 11.11

(E) CATCH BASIN
 GRATE=13.60 N 13.57 S
 8" PVC INV(SE)= 11.92
 8" PVC OUT(NW)= 11.81

STORM INLET DRAIN PROTECTION
 ESC-2

STORM INLET DRAIN PROTECTION
 ESC-2

4
 ESC-2

COQUILLE RIVER

RELOCATE RAIL BARRIER
 SEE SHIT C-1

LIGHT AND SIGN

WOODEN SIDEWALK


BENCH AND SIGN AREA

PLAN
 1"=10'

REGISTERED PROFESSIONAL ENGINEER
 15,479
 Daniel P. Graber
 OREGON
 JULY 25, 1991
 DANIEL P. GRABER
 RENEWS 06/30/21

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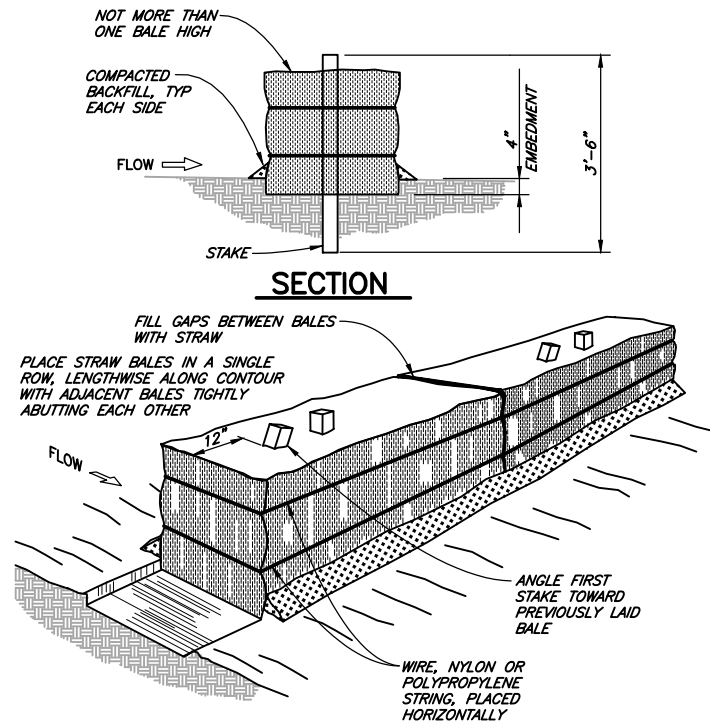
DSGN
 DR
 CHK
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PORT OF BANDON
 MARKET PARKING IMPROVEMENT PLAN
 BANDON, OREGON

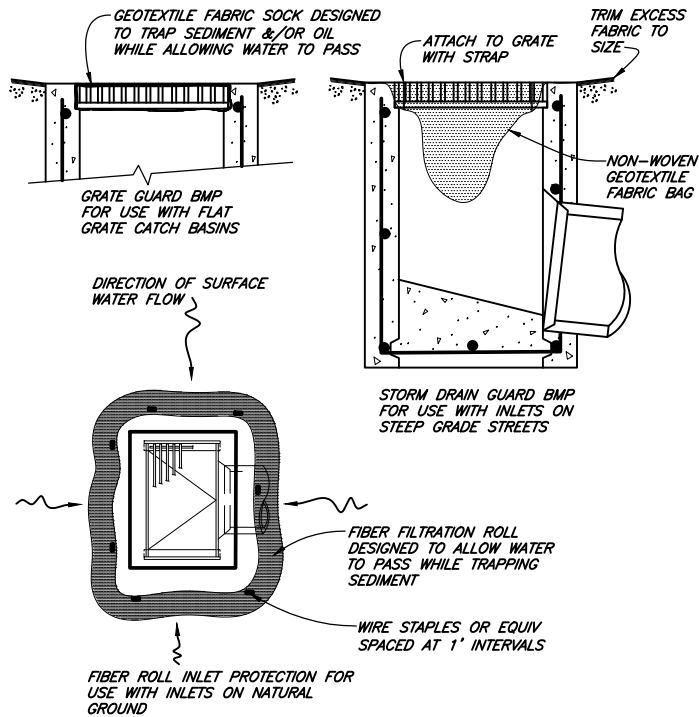
DEMOLITION & ESC PLAN

SHEET
 ESC-1

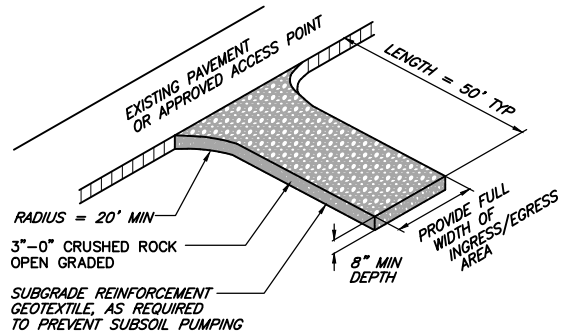
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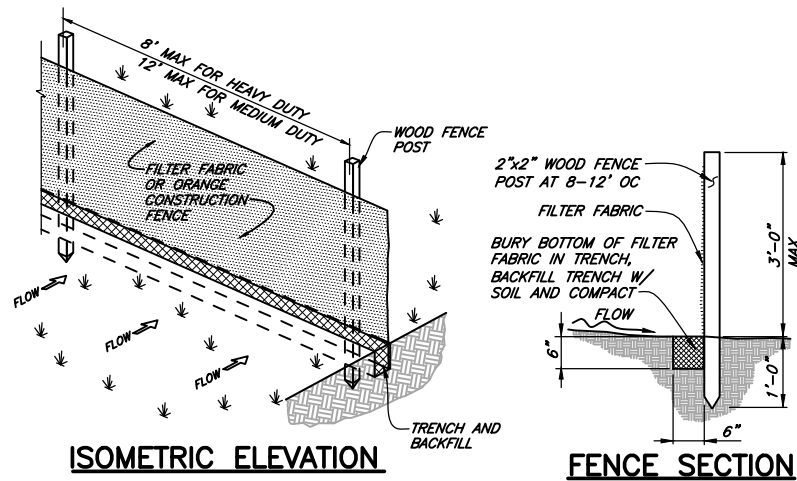
DETAIL 1
NTS
(STRAW BALE BARRIER)



DETAIL 2
NTS
(STORM DRAIN INLET PROTECTION)



DETAIL 3
NTS
(GRAVEL CONSTRUCTION EXIT)



NOTES FOR SILT FENCE:

1. THE FILTER FABRIC SHALL BE PURCHASED IN A CONTINUOUS ROLL CUT TO THE LENGTH OF THE BARRIER TO AVOID USE OF JOINTS. WHEN JOINTS ARE NECESSARY, FILTER CLOTH SHALL BE SPLICED TOGETHER ONLY AT A SUPPORT POST, WITH A MINIMUM 6-INCH OVERLAP, AND BOTH ENDS SECURELY FASTENED TO THE POST.
2. THE FILTER FABRIC SHALL HAVE A MINIMUM VERTICAL BURIAL OF 6 INCHES.
3. STANDARD OR HEAVY DUTY FILTER FABRIC FENCE SHALL HAVE MANUFACTURED STITCHED LOOPS FOR 2-INCH x 2-INCH POST INSTALLATIONS. STITCHED LOOPS SHALL BE INSTALLED ON THE UP HILL SIDE OF THE SLOPED AREA.
4. WHEN NO LONGER REQUIRED, FILTER FABRIC FENCES SHALL BE REMOVED AND PROPERLY DISPOSED OF.

DETAIL 4
NTS
(SILT FENCE OR ORANGE CONSTRUCTION FENCE)

EROSION CONTROL NOTES:

1. EROSION CONTROL BMP'S SHALL BE INSTALLED PRIOR TO ANY SITE DISTURBANCE AND MAINTAINED SUCH THAT NO VISIBLE SEDIMENT LEAVES THE SITE.
2. THE CONSTRUCTION, MAINTENANCE, REPLACEMENT AND UPGRADING OF THESE BMP'S IS THE RESPONSIBILITY OF THE APPLICANT/CONTRACTOR UNTIL ALL CONSTRUCTION IS COMPLETED AND APPROVED AND PERMANENT VEGETATION/LANDSCAPING IS ESTABLISHED.
3. BMP'S SHOWN ON THESE PLANS MAY BE CONSTRUCTED, ADJUSTED, OR MODIFIED IN CONJUNCTION WITH CLEARING AND GRADING ACTIVITIES AS REQUIRED TO INSURE THAT SEDIMENT AND SEDIMENT LADEN WATER DO NOT ENTER THE DRAINAGE SYSTEM, ROADWAYS, OR VIOLATE APPLICABLE WATER STANDARDS.
4. TEMPORARY SEDIMENT BASINS SHALL NOT EXCEED THE FINAL DEPTH OF THE RAIN GARDENS.
5. CONTRACTOR MAY SCHEDULE AN IN-FIELD PRE-CONSTRUCTION MEETING WITH THE ENGINEER BEFORE COMMENCING WORK TO REVIEW EROSION CONTROL MEASURES.
6. PAVED AREAS AT THE ACCESS POINTS SHALL BE SWEEPED OR VACUUMED AS OFTEN AS EACH DAY TO ELIMINATE TRACKING SOIL AND DEBRIS BEYOND THE LIMITS OF THE PROJECT SITE. STREET WASHING IS NOT ALLOWED. ANY SOILS AND/OR DEBRIS, ROCK, GRAVEL, ETC. TRACKED BEYOND THE LIMITS OF THE PROJECT SITE AS A RESULT OF THIS PROJECT SHALL BE REMOVED IMMEDIATELY.
7. INLET PROTECTION TO REMAIN IN PLACE UNTIL SURFACING IS COMPLETED, STOCKPILES ARE REMOVED, AND VEGETATION IS RE-ESTABLISHED.
8. PROVIDE INLET PROTECTION ON ALL NEW CATCH BASINS DURING CONSTRUCTION.

MAINTENANCE AND INSPECTION:

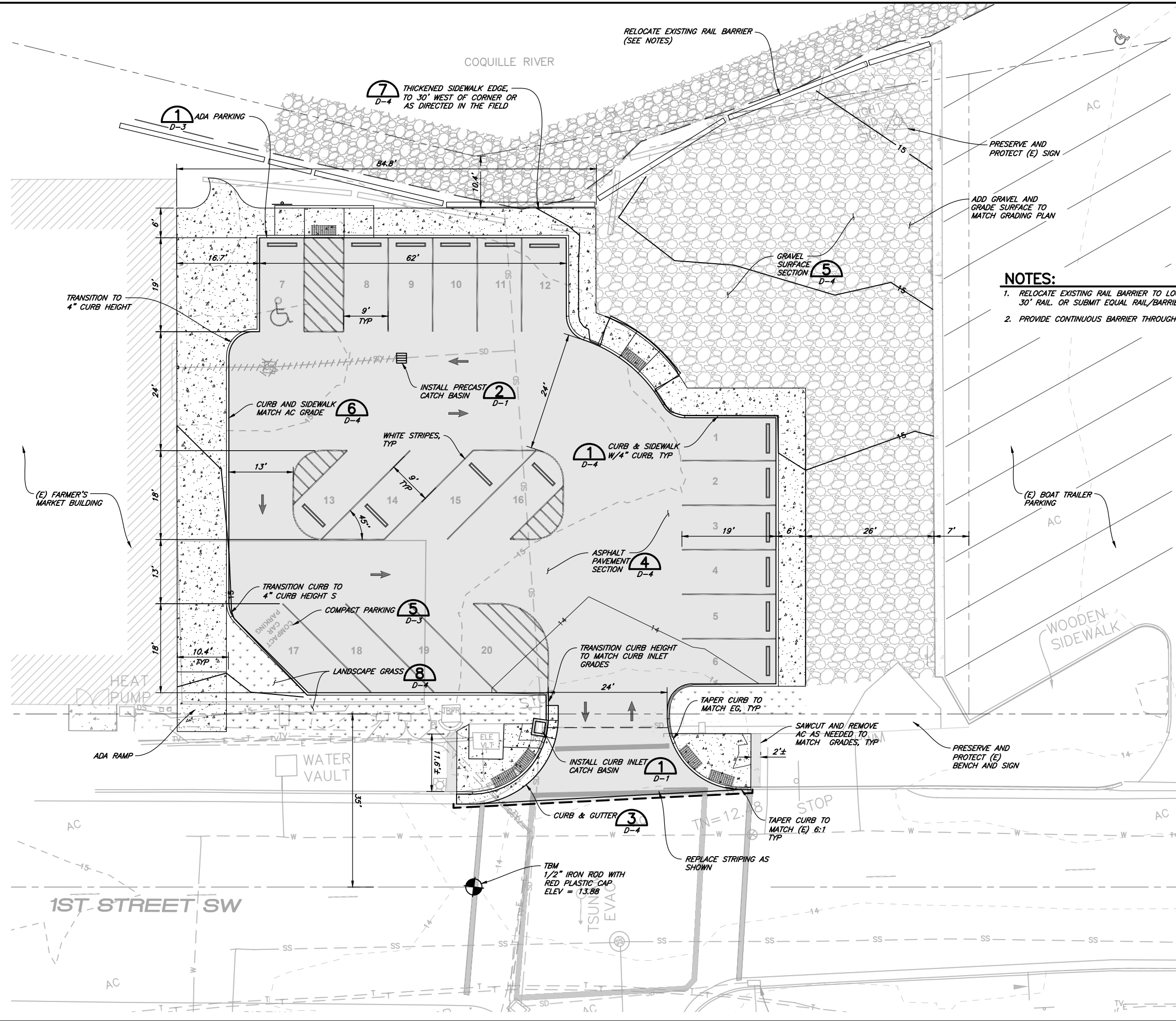
9. MAINTENANCE AND INSPECTION OF BMP'S, AT A MINIMUM, SHALL BE CONDUCTED ACCORDING TO THE FOLLOWING SCHEDULE:
 - BMP'S SHALL BE INSPECTED DAILY BY THE CONTRACTOR AND MAINTAINED AS NECESSARY TO ENSURE THEIR CONTINUED FUNCTIONING.
 - BMP'S AT INACTIVE SITES SHALL BE INSPECTED AND MAINTAINED A MINIMUM OF ONCE A MONTH, PRIOR TO A FORECAST STORM, AND WITHIN 24 HOURS FOLLOWING A STORM EVENT.
 9. SOIL STOCKPILES SHALL BE COVERED, STABILIZED, OR PROTECTED WITH SOIL STABILIZATION MEASURES AND A PERIMETER SEDIMENT BARRIER AT ALL TIMES.
 10. STOCKPILES OF CONTAMINATED SOIL SHALL BE MANAGED IN ACCORDANCE WITH CASQA BMP HANDBOOK FOR "CONTAMINATED SOIL MANAGEMENT".
- DEWATERING:**
11. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR THE PROPER DESIGN, INSTALLATION, OPERATION, AND DESTRUCTION OF DEWATERING FACILITIES NEEDED DURING CONSTRUCTION.
 12. CONTRACTOR SHALL ESTABLISH AND MAINTAIN DEWATERING FACILITIES TO ALLOW FOR THE EXCAVATION, AND SUBSEQUENT PLACEMENT AND RECOMPACTION OF TRENCH MATERIAL WITHIN THE EXCAVATED AREA.
 13. HANDLING OF WATER FROM THE EXCAVATION AND DISPOSAL OF SAME FROM THE PROJECT SITE SHALL BE PERFORMED IN ACCORDANCE WITH BMP'S TO AVOID SEDIMENT TRANSPORT AND OTHER IMPACTS TO RECEIVING WATERS.



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CHK	DATE
APVD	NO.
PORT OF BANDON PARKING IMPROVEMENT PLAN BANDON, OREGON	
ESC DETAILS	
SHEET ESC-2	
SEQ	
DATE 4/2021	
PROJ. NO. 618074	

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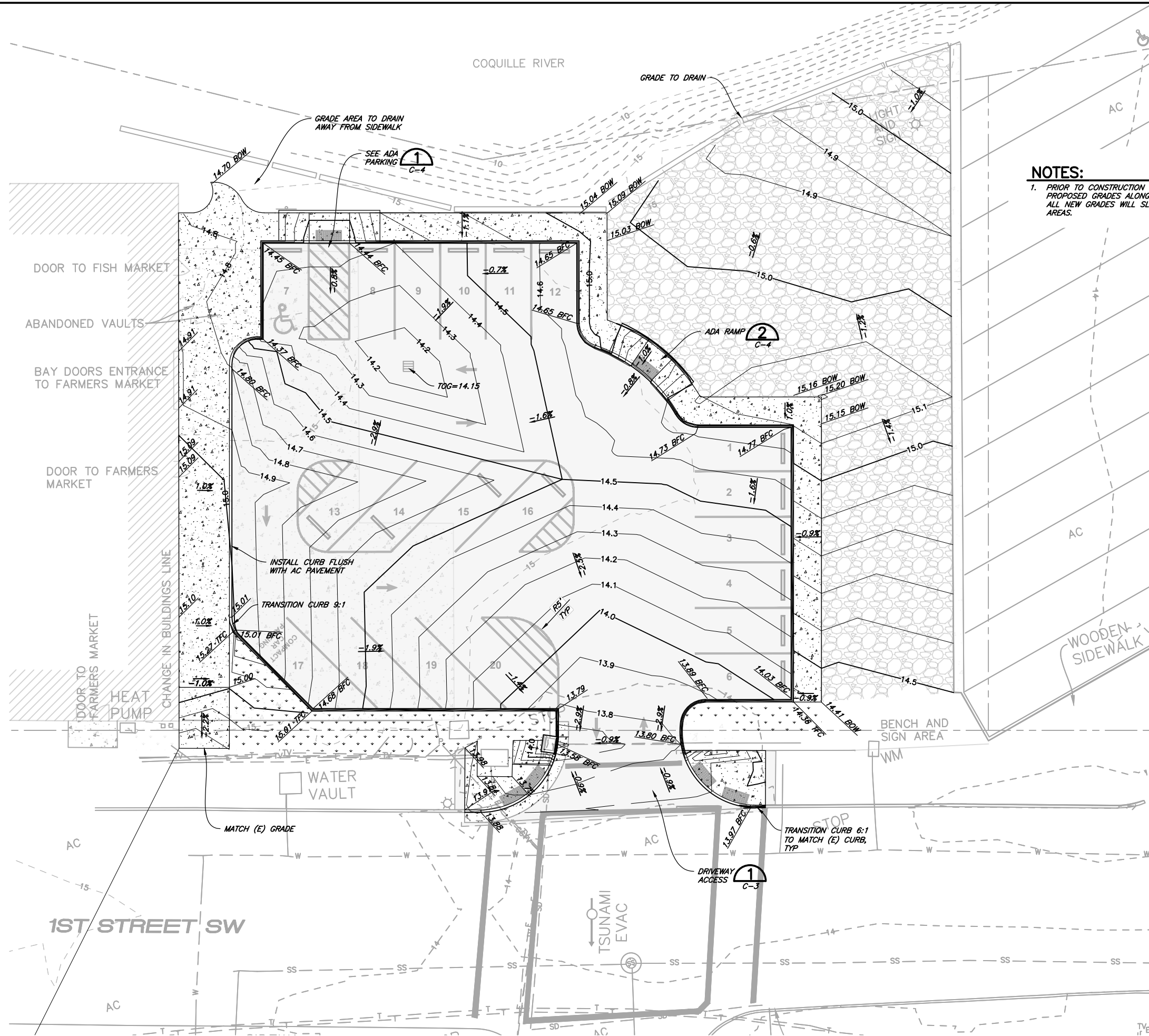
- NOTES:**
1. RELOCATE EXISTING RAIL BARRIER TO LOCATION SHOWN. ADD ONE ADDITIONAL 30' RAIL. OR SUBMIT EQUAL RAIL/BARRIER PROTECTION SYSTEM FOR APPROVAL.
 2. PROVIDE CONTINUOUS BARRIER THROUGHOUT CONSTRUCTION.

PLAN
 1"=10'



VERIFY SCALES BAR IS ONE INCH ON ORIGINAL DRAWING IF NOT ONE INCH ON THIS SHEET, INDUSTRY SCALES ACCORDINGLY	
275 MARKET AVENUE COOS BAY, OR 97420 WWW.SPA-ENG.COM 541-266-9880	
SPA	
NO.	DATE
REVISION	BY
APVD	RFS
CHK	DSR
DR	DPG
DESIGN	
PORT OF BANDON PARKING IMPROVEMENT PLAN BANDON, OREGON	
SITE PLAN	
SHEET C-1	
SEQ	
DATE 4/2021	
PROJ. NO. 618074	

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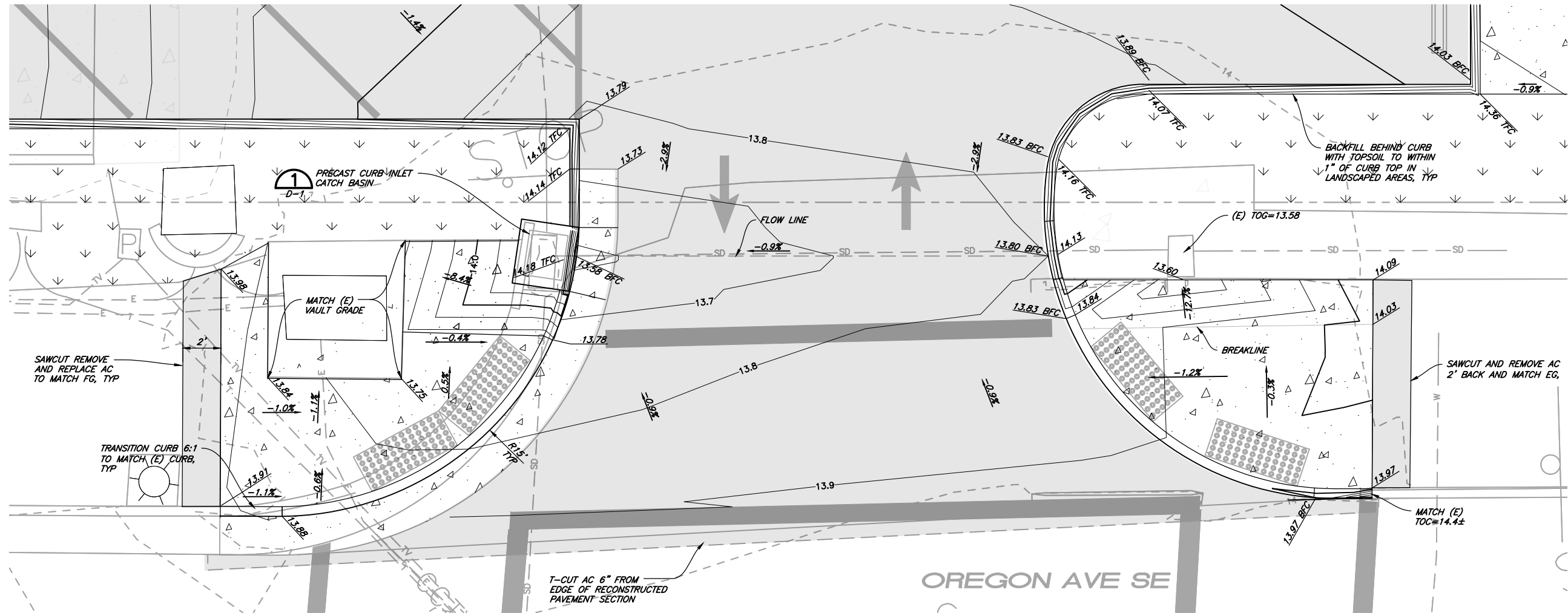
NOTES:

1. PRIOR TO CONSTRUCTION CONTRACTOR SHALL FIELD VERIFY EXISTING AND PROPOSED GRADES ALONG THE EXISTING BUILDING FRONTAGE AND VERIFY THAT ALL NEW GRADES WILL SLOPE AWAY FROM THE BUILDING TOWARD DRAINAGE AREAS.



VERIFY SCALES B.B. IS ONE INCH ON ORIGINAL DRAWING O IF NOT ONE INCH ON THIS SHEET, INDICATE SCALES ACCORDINGLY	
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CHK	DSR
DR	DPG
PORT OF BANDON MARKET PARKING IMPROVEMENT PLAN BANDON, OREGON	
GRADING PLAN	
SHEET C-2	
DATE 4/2021	
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DETAIL 1
 NTS
 C-2
 (DRIVEWAY ACCESS)



VERIFY SCALES
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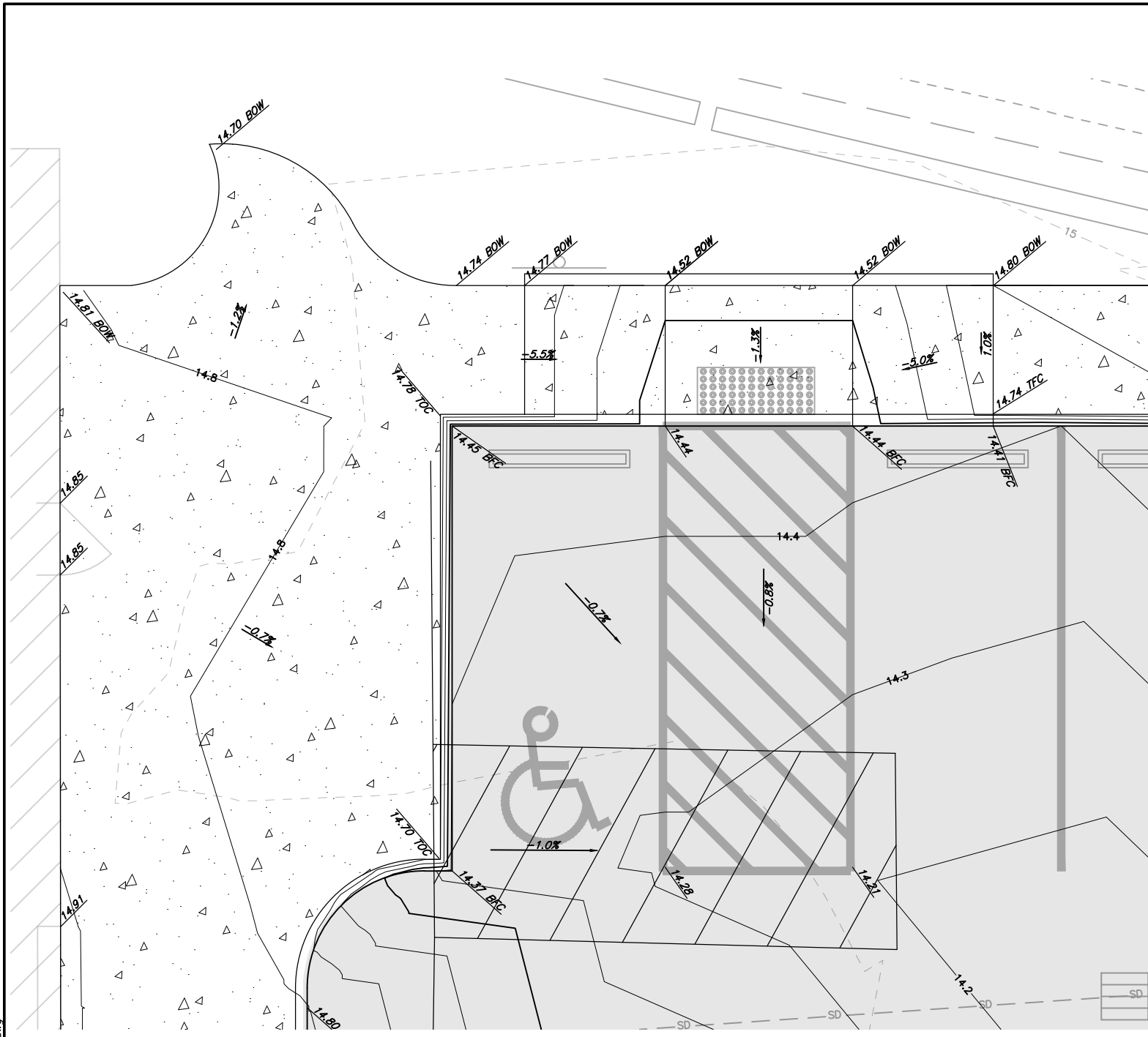


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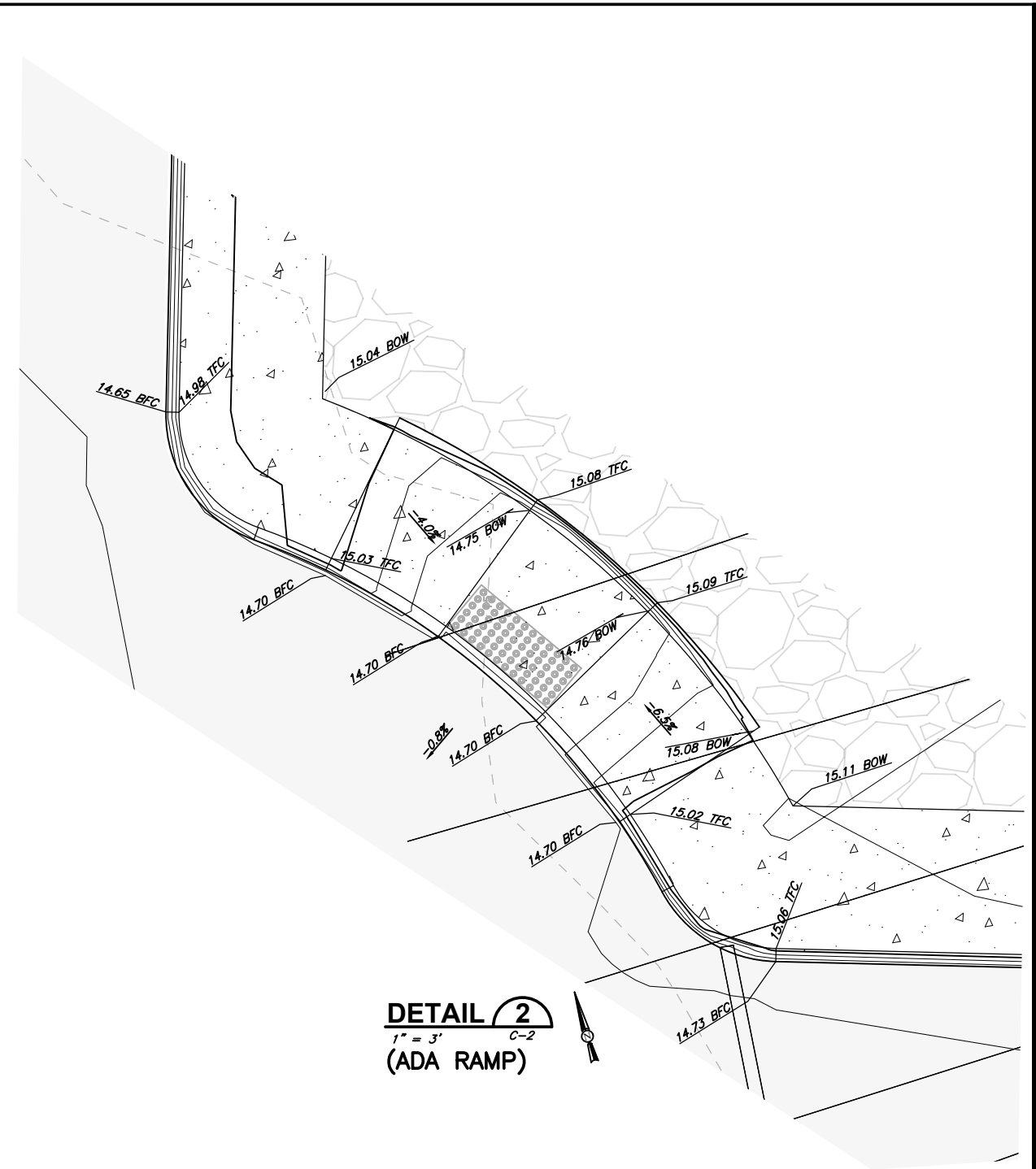
PORT OF BANDON
 MARKET PARKING IMPROVEMENT PLAN
 BANDON, OREGON
GRADING PLAN DETAILS

SHEET
C-3
 SEQ
 DATE 4/2021
 PROJ. NO.
 618074

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DETAIL 1
 1" = 3'
 C-2
 (ADA PARKING)



DETAIL 2
 1" = 3'
 C-2
 (ADA RAMP)

REGISTERED PROFESSIONAL
 ENGINEER
 15,479
 Daniel P. Graber
 OREGON
 JULY 25, 1991
DANIEL P. GRABER
 RENEWS 06/30/21

VERIFY SCALES
 0 IF NOT ONE INCH ON ORIGINAL DRAWING
 1 IF NOT ONE INCH ON THIS SHEET (POST SCALES ACCORDINGLY)

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SPA

NO.	DATE	REVISION	BY

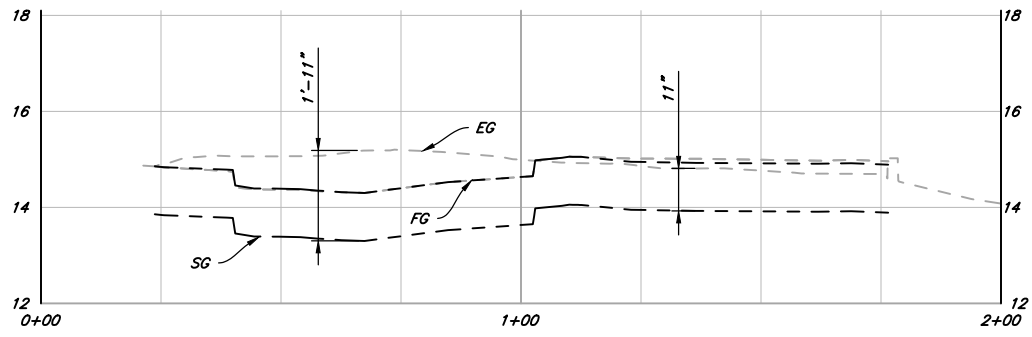
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DPG	DSR	RFS

PORT OF BANDON
 MARKET PARKING IMPROVEMENT PLAN
 BANDON, OREGON

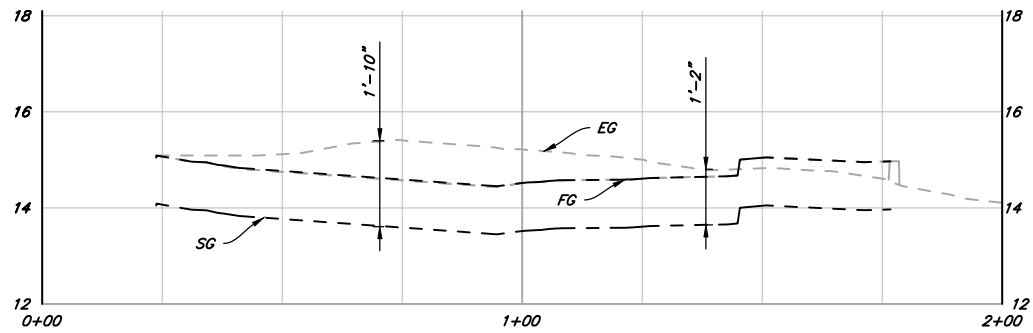
GRADING PLAN DETAILS

SHEET C-4
 SEQ
 DATE 4/2021
 PROJ. NO. 618074

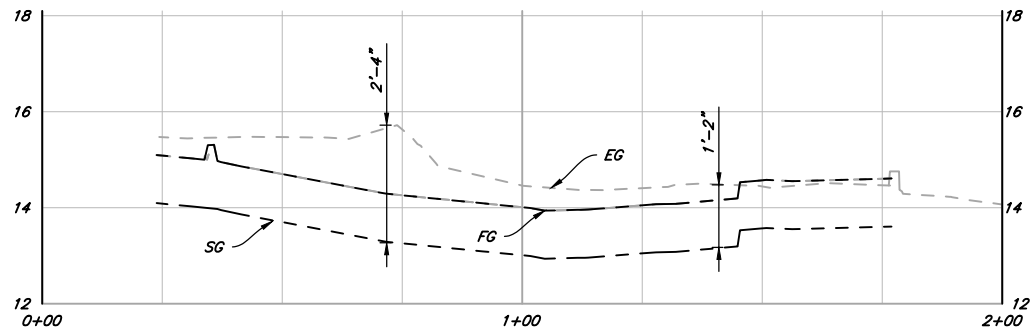
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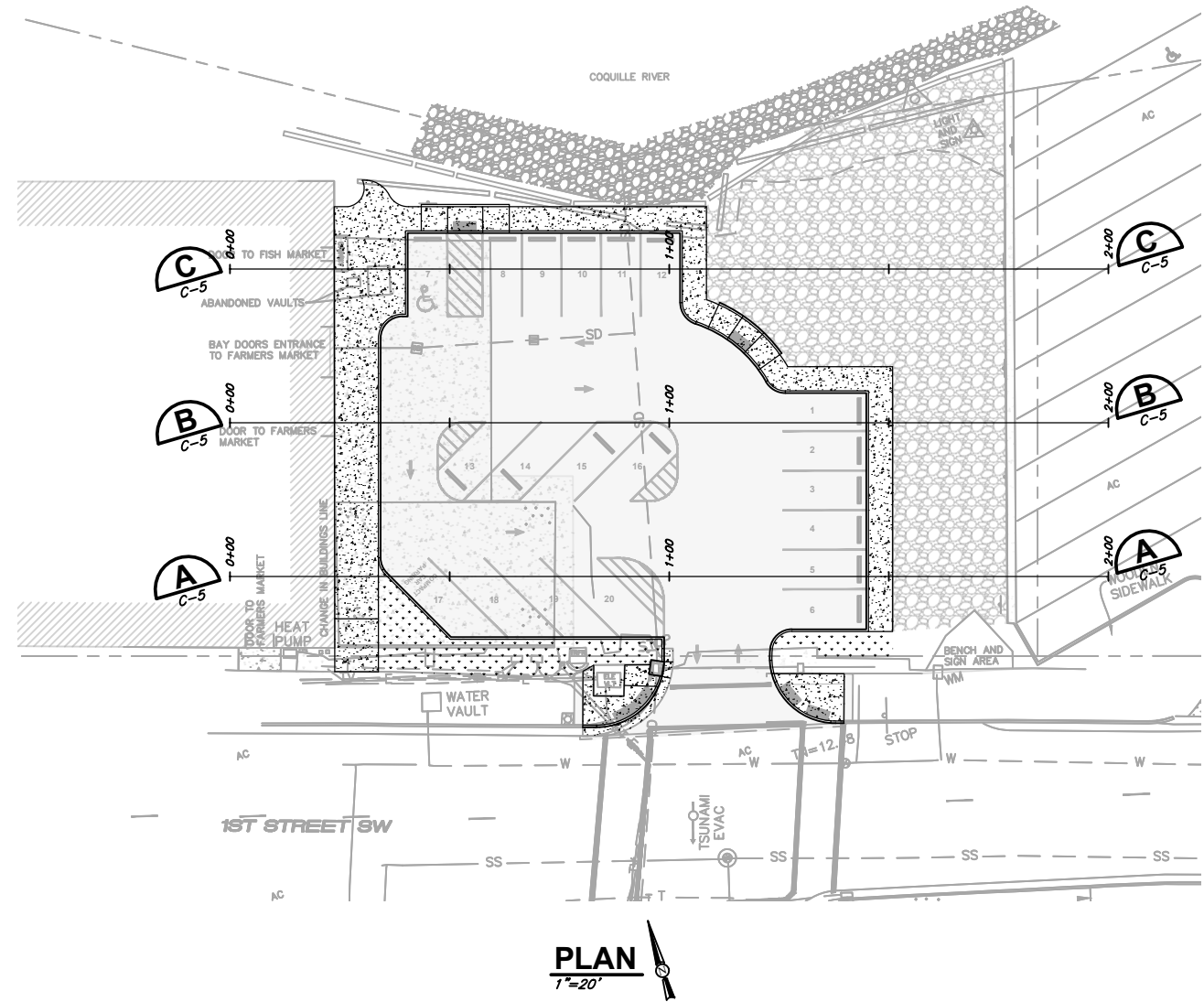
PROFILE C
 SCALE: 1"=20' H
 1"=2' V C-5



PROFILE B
 SCALE: 1"=20' H
 1"=2' V C-5



PROFILE A
 SCALE: 1"=20' H
 1"=2' V C-5



PLAN
 1"=20'



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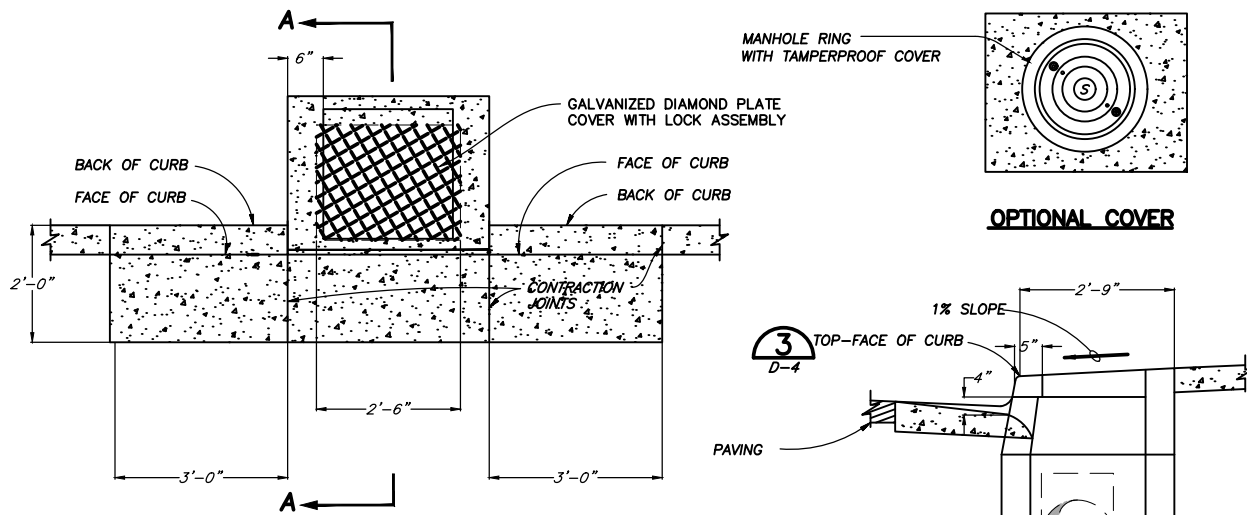
APVD	CHK	RFS	DSR	DFG

PORT OF BANDON
 MARKET PARKING IMPROVEMENT PLAN
 BANDON, OREGON

SHEET	C-5
SEQ	
DATE	4/2021
PROJ. NO.	618074

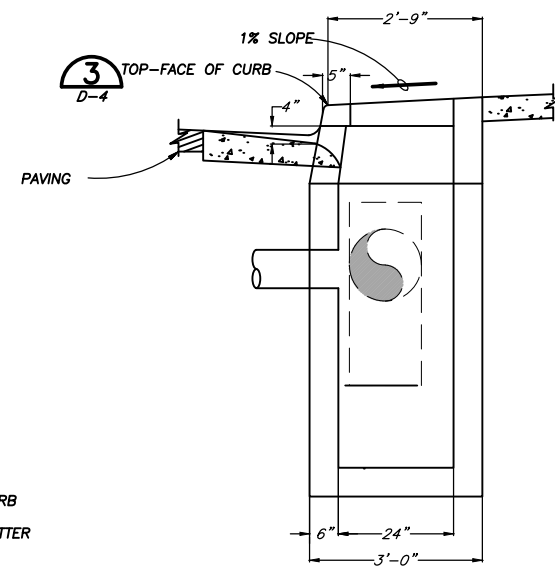
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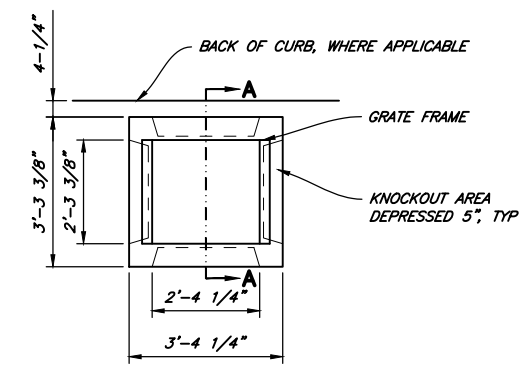


PLAN VIEW

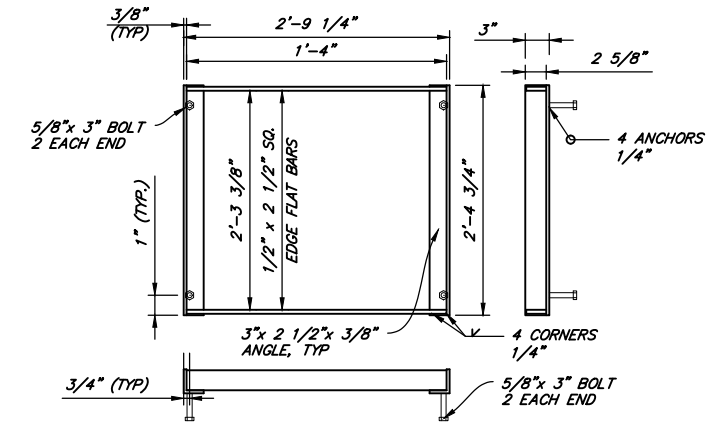
OPTIONAL COVER



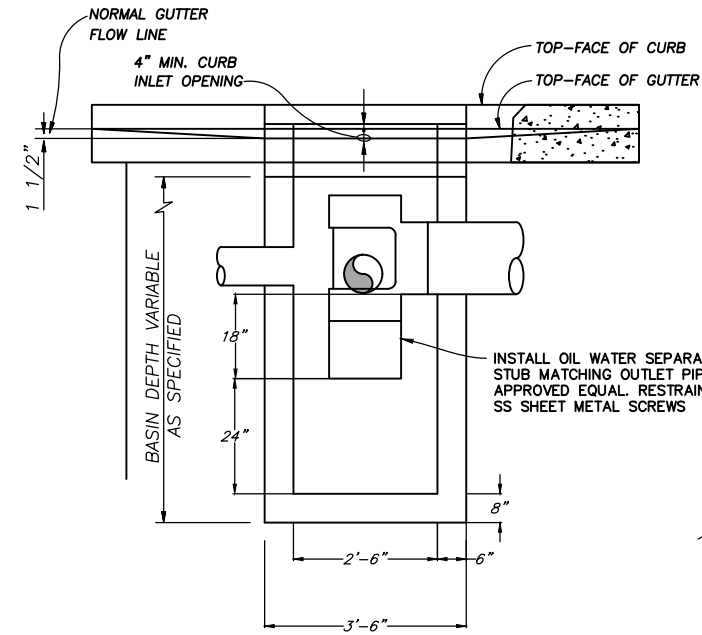
SECTION A-A



PLAN

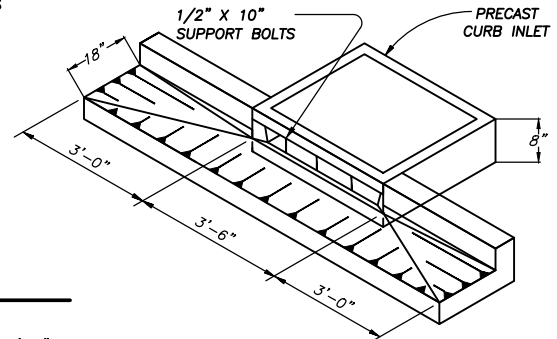


CG-2 FRAME



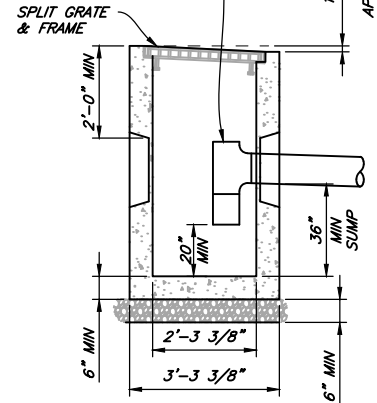
FRONT VIEW

INSTALL OIL WATER SEPARATOR TEE WITH STUB MATCHING OUTLET PIPE SIZE, OR APPROVED EQUAL. RESTRAIN JOINTS WITH 3 SS SHEET METAL SCREWS

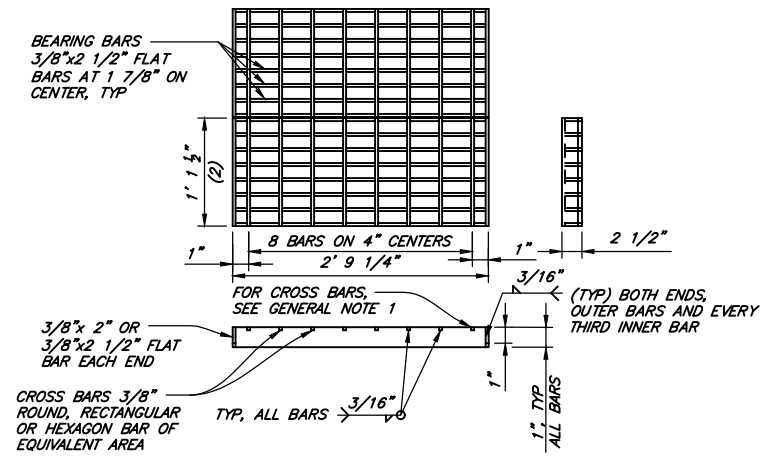


ISOMETRIC VIEW

INSTALL OIL WATER SEPARATOR TEE WITH STUB TO MATCH OUTLET PIPE SIZE, OR EQUAL. RESTRAIN JOINTS WITH 3 SS SCREWS OR EQUAL



SECTION A-A



G-2 TYPE 2 GRATE
(BICYCLE-SAFE)
(2 GRATES REQUIRED PER INLET)

NOTES:

1. CURB INLET TOP AND BASE SHALL MEET H20 LOADING.
2. CURB INLET BASE MAY BE PRECAST OR CAST-IN-PLACE.
3. FOR STEEP GRADES USE STD. PRECAST INLET WITH 4'-0" OPENING OR TWO 2'-6" OPENING INLETS.
4. CONCRETE STRENGTH SHALL BE 3000 PSI.
5. ALL FABRICATED METAL PARTS SHALL BE HOT-DIPPED GALVANIZED AFTER FABRICATION.
6. REFERENCE ODOT RD371 INLET BASE AND 372 OR 373 INLET TOP

NOTES:

1. GRATE AND FRAME MAY EACH BE OF CAST IRON OR WELDED STEEL CONSTRUCTION.
2. FOR PRECAST BOX, CURB MUST BE HAND FORMED 10' EACH SIDE OF CATCH BASIN.
3. CONCRETE STRENGTH SHALL BE 3,000 PSI.
4. CATCH BASIN AND GRATE SHALL MEET HS20 LOADING, REFERENCE ODOT RD364 & RD365.
5. CONSTRUCT A SLUMP DEPTH AS SHOWN ON PLANS.
6. OUTLET PIPE SHALL HAVE A MINIMUM OF 18" OF COVER.
7. WHEN RIGID PIPE IS USED, THE CONNECTING PIPE SHALL HAVE A FLEXIBLE, GASKETED AND UNRESTRAINED JOINT WITHIN 18" OF STRUCTURE WALL.
8. WHEN FLEXIBLE PIPE IS USED, INSTALL RESILIENT CONNECTORS CONFORMING TO REQUIREMENTS OF ASTM C923 (KOR-N-SEAL BOOT).
9. BACKFILL WITH 3/4"-0 BASE ROCK COMPACTED TO 95% RELATIVE MAXIMUM DENSITY.

NOTES:

1. 3/8" CROSS BARS SHALL BE FLUSH WITH THE TOP OF GRATE SURFACE AND MAY BE FILLET WELDED, RESISTANCE WELDED OR ELECTROFORGED TO BEARING BARS.
2. HOT DIP GALVANIZE AFTER FABRICATION.
3. CAST IRON GRATE AND FRAME ARE ACCEPTABLE ALTERNATES. SEE ODOT'S QUALIFIED PRODUCTS LIST.

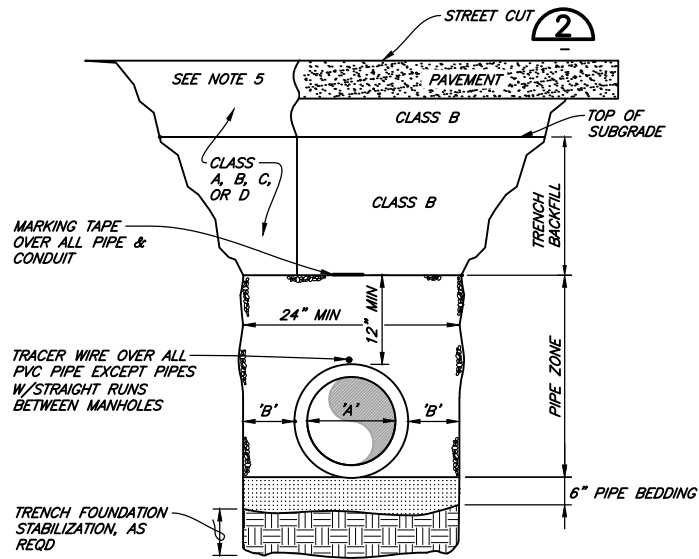
DETAIL 1
(PRECAST CURB INLET CATCH BASIN)

DETAIL 2
(STANDARD PRECAST CATCH BASIN WITH FRAME GRATE)



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BY	
REVISION	
DATE	
NO.	
DSGN DFG DR DSR CHK RFS APVD	
PORT OF BANDON MARKET PARKING IMPROVEMENT PLAN BANDON, OREGON	
DRAINAGE DETAILS	
SHEET	D-1
SEQ	
DATE	4/2021
PROJ. NO.	618074

DIM TABLE	
"A" (IN)	"B" (IN)
4	10
6	10
8	10
10	10
12	12
15	12
18	16
21	16
24	18
30	18
36	24
42	24
48	24
54	24
60	24
66	24
72	24



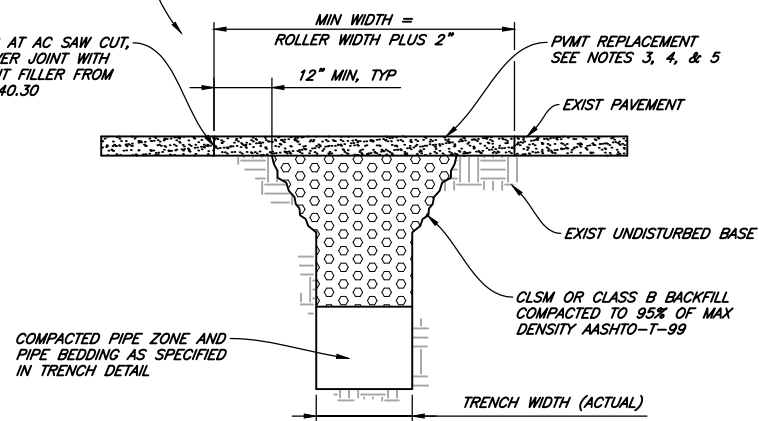
NOTES:

- FOUNDATION STABILIZATION SHALL BE PROVIDED WHEN MATERIAL AT THE TRENCH BOTTOM IS UNSUITABLE TO PROVIDE A STABLE TRENCH BASE.
- GRAVEL SURFACES SHALL BE REPLACED W/CLASS B BASEROCK TO MATCH EXISTING GRAVEL SECTION.
- PIPE BEDDING AND PIPE ZONE SHALL BE CLASS B EXCEPT CONDUIT ON SITE LESS THAN 4" MAY BE CLASS C.
- TAPE TRACER WIRE TO PIPE EVERY 5'.
- FINISHED SURFACE MATERIALS TO BE PROVIDED WITH SITE DEVELOPMENT.
- REFERENCE ODOT STANDARD DETAIL RD 300, "TRENCH".

DETAIL 1
NTS
(TRENCH)

SAW CUT AC. SEAL AC JOINT WITH "EMULSIFIED ASPHALT," RVS-1 OR RS-2 GRADE, ASTM D-977 OR APPROVED EQUAL.

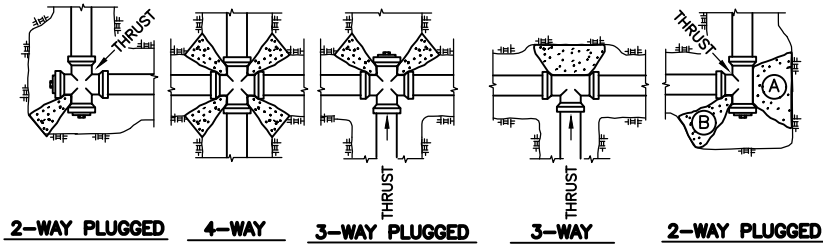
TACK COAT EDGES AT AC SAW CUT, SEAL SURFACE OVER JOINT WITH HOT POURED JOINT FILLER FROM OREGON QPL 02440.30



NOTES:

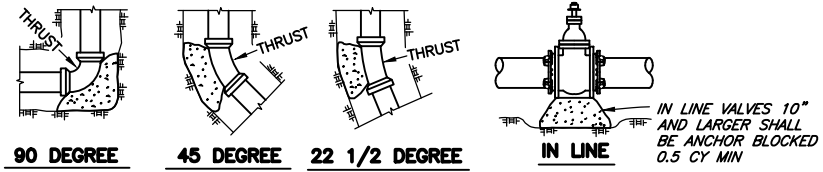
- ALL EXISTING AC OR PCC PAVEMENT SHALL BE SAWCUT PRIOR TO REPAVING.
- CONCRETE PAVEMENT SHALL BE REPLACED WITH CONCRETE TO A MINIMUM THICKNESS OF 6" OR TO THE THICKNESS OF REMOVED PAVEMENT, WHICHEVER IS GREATER.
- SMOOTH 1" DOWEL BARS SHALL BE INSTALLED ON 12-INCH CENTERS.
- PLACE AC MIX MINIMUM THICKNESS OF 4" OR THE THICKNESS OF THE REMOVED PAVEMENT, WHICHEVER IS GREATER. COMPACT AS SPECIFIED. AC PAVEMENT GREATER THAN 3" SHALL BE PLACED IN LIFTS OF EVEN THICKNESS. MAX 2" PER LIFT.
- IN SITUATIONS WHERE EXISTING PCC PAVEMENT IS OVERLAID WITH AC PAVEMENT, PLACE PCC PAVEMENT IN ACCORDANCE WITH NOTE 3.
- REFERENCE ODOT STANDARD DETAIL RD 302, "STREET CUT".

DETAIL 2
NTS
(STREET CUT)

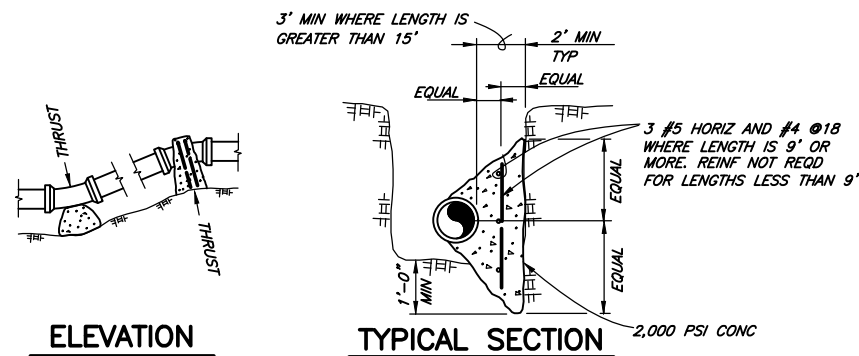


CROSSES

TEES



BENDS



ELEVATION

TYPICAL SECTION

SQ FT OF THRUST BLOCK BEARING AREA

PIPE SIZE	TEE, WYE, DEAD END & HYDRANT	90° BEND PLUGGED CROSS	TEE PLUGGED ON RUN		45° BEND	22 1/2° BEND	11 1/4° BEND
			(A)	(B)			
4"	2	3	2	3	2	1	1
6"	5	6	5	6	4	2	1
8"	8	11	8	11	6	3	2
10"	12	17	12	17	9	5	3
12"	17	24	17	24	13	7	4
14"	23	33	23	33	18	9	5
16"	31	43	31	43	24	12	6
18"	40	55	40	55	30	16	8
20"	50	70	50	70	37	19	10
24"	70	100	70	100	52	27	14

NOTES:

- ALL WATER PIPING TO BE INSTALLED WITH RESTRAINED JOINT SYSTEM AND THRUST BLOCKS FOR A 200% RESTRAINED JOINT SYSTEM. SEE RESTRAINED JOINT TABLE FOR LENGTH OF PIPE TO BE RESTRAINED FOR VARIOUS PIPE DIAMETERS AND FITTINGS.
- CONCRETE THRUST BLOCKS SHALL BE POURED AGAINST FIRM, UNDISTURBED EARTH OR STRUCTURAL BACKFILL.
- CONCRETE SHALL BE KEPT CLEAR OF ALL JOINTS AND ACCESSORIES.
- VOLUMES AND SPECIAL BLOCKING DETAILS SHOWN ON THE PLANS TAKE PRECEDENCE OVER VOLUMES AND BLOCKING DETAILS SHOWN ON THIS STANDARD DETAIL.
- ALL BURIED PIPE EXCEPT FLANGED, SCREWED, SOLVENT-WELDED PVC, FUSION-BONDED PEP OR WELDED STEEL PIPE SPECIFIED TO BE PRESSURE TESTED SHALL BE PROVIDED WITH CONCRETE THRUST BLOCKS AT ALL DIRECTIONAL CHANGES UNLESS OTHERWISE NOTED.
- THRUST BLOCKS SHALL NOT BE LOCATED OR SIZED TO ENCASE ADJACENT PIPES OR FITTINGS.
- THE SIZE AND WEIGHT OF ALL UPLIFT THRUST BLOCKS SHALL BE DETERMINED BY THE ENGINEER.
- THE BEARING AREAS ARE BASED ON TEST PRESSURE OF 150 PSI AND ALLOWABLE SOIL BEARING STRESS OF 1000 POUNDS PER SQUARE FOOT. TO COMPUTE BEARING AREAS FOR DIFFERENT TEST PRESSURES AND SOIL BEARING STRESSES USE THE FOLLOWING EQUATION: BEARING AREA= (TEST PRESSURE/150) X (1000/ALLOWABLE SOIL BEARING STRESS) X (TABLE VALUE).
- REFERENCE ODOT STANDARD DRAWING RD 250 THRUST BLOCKING.

DETAIL
NTS
(THRUST BLOCKING)

RESTRAINED PIPE LENGTH TABLE

FITTING	RESTRAINED LENGTH					
	18"	16"	12"	8"	6"	4"
END	145'	131'	102'	72'	55'	39'
90° ELBOW	46'	42'	33'	24'	18'	13'
45° ELBOW	19'	18'	14'	10'	8'	6'
22.5° ELBOW	9'	9'	7'	5'	4'	3'
11.25° ELBOW	5'	5'	4'	3'	2'	2'
8"x6" TEE				R-5'	B-14'	
TEE, SIZE ON SIZE	R-20' B-13'	R-20' B-11'	R-16' B-11'	R-12' B-11'	R-9' B-11'	R-7' B-11'

RESTRAINED LENGTH CALCULATIONS BASED ON EBAA IRON RESTRAINT LENGTH CALCULATOR AND THE FOLLOWING CONDITIONS: PIPE MATERIAL PVC, SOIL TYPE SM, SAFETY FACTOR 1.5, TRENCH TYPE 5, DEPTH OF TRENCH 3', TEST PRESSURE 150 PSI

R=RUN B=BRANCH

RESTRAINED PIPE LENGTH TABLE NOTES:

- PROVIDE MECHANICAL JOINT RESTRAINT FOR ALL FITTINGS AND POINTS WITH UNBALANCE FORCES, TOGETHER WITH BELL HARNESSSES AT ALL JOINTS WITHIN THE RESTRAINED JOINT LENGTH REQUIRED, PURSUANT TO THE RESTRAINED JOINT TABLE.
- THE RESTRAINED JOINT LENGTHS LISTED IN THE TABLE ARE ONLY VALID FOR THE CONDITIONS LISTED IN THE TABLE. CHANGES TO THE CONDITIONS LISTED MAY REQUIRE ADJUSTMENTS TO THE RESTRAINED LENGTH REQUIRED.
- PIPE BEDDING AND PIPE ZONE MATERIALS AROUND AND ON EACH SIDE OF THE RESTRAINED PIPE SHALL BE 3/4"-0 COMPACTED TO 95% RELATIVE DENSITY TEST FOR THE MINIMUM DISTANCE SHOWN IN THE JOINT RESTRAINT TABLE.
- COMPACTION TESTS SHALL BE TAKEN AT LOCATIONS DIRECTED BY THE ENGINEER FOR EACH FITTING.
- WHERE RESTRAINT LENGTH CANNOT BE MET OR WHERE TEMPORARY PLUGS OR FITTINGS ARE REQUIRED, PROVIDE THRUST BLOCKS TO PREVENT PIPE MOVEMENT.
- IN ADDITION TO MECHANICALLY RESTRAINED JOINTS, PROVIDE THRUST BLOCKS FOR ALL FITTINGS AND ALL POINTS WITH UNBALANCED FORCES. EACH THRUST RESTRAINT SYSTEM IS TO BE CONSTRUCTED TO PERFORM INDEPENDENTLY PROVIDING A DUAL RESTRAINT SYSTEM FOR THE PIPE.
- REFERENCE EBAA IRON RESTRAINT LENGTH CALCULATOR.



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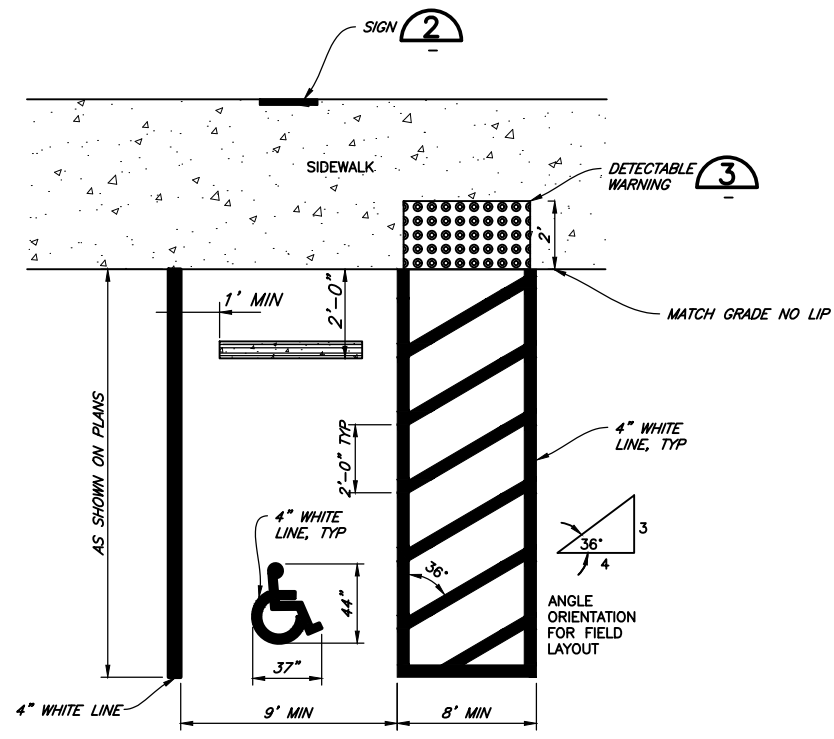
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APVD	RFS	CHK	DSR	DPG

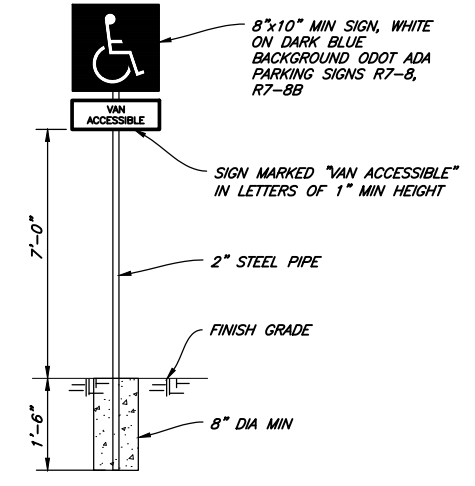
PORT OF BANDON
 MARKET PARKING IMPROVEMENT PLAN
 BANDON, OREGON
TRENCH DETAILS

SHEET	D-2
SEQ	
DATE	4/2021
PROJ. NO.	618074

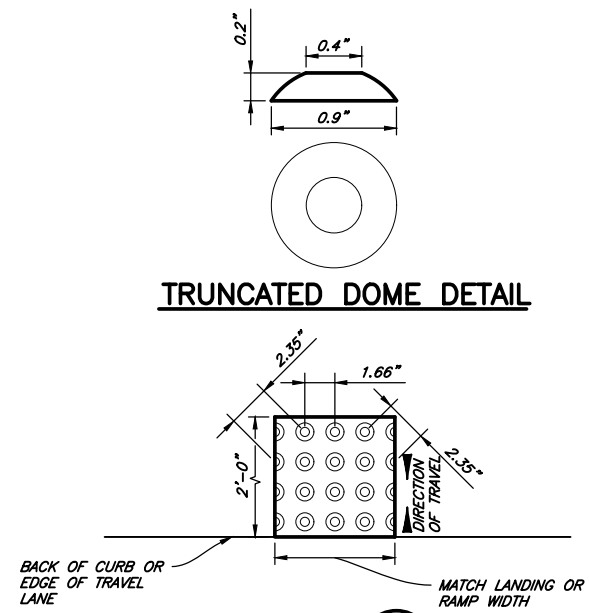
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DETAIL 1
NTS C-1
(ADA PARKING)



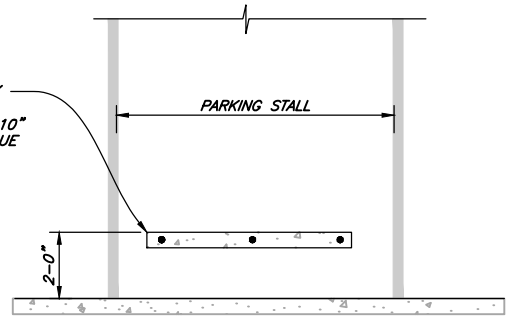
DETAIL 2
NTS
(ADA SIGN)



DETAIL 3
NTS
(DETECTABLE WARNING PATTERN)

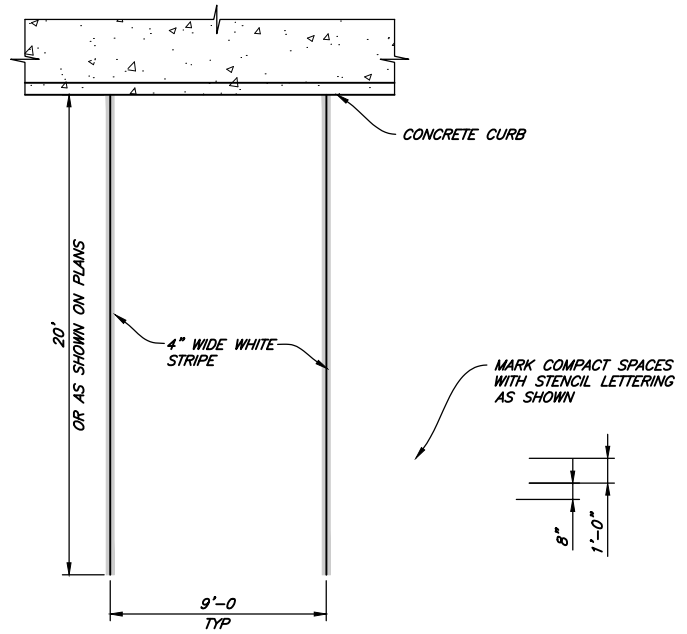
- NOTES:**
- 8-FT. MINIMUM AISLE REQUIRED FOR VAN-ACCESSIBLE SPACES.
 - MOUNT R7-B (ON TOP) AND R7-BB (BOTTOM) SIGNS ON POST OR WALL.
 - WALL MIN 7-FT. CLEARANCE BETWEEN BOTTOM OF R7-BB SIGN AND SIDEWALK.
 - MARKING PAINT SHALL BE AS LISTED AS APPROVED IN THE ODOT QUALIFIED PRODUCTS LIST (QPL) AND INSTALLED PER MANUFACTURER'S RECOMMENDATIONS.

REMOVABLE PARKING BLOCK TRAFFIC SAFETY STORE MODEL: COMMERCIAL, OR EQUAL 70.5"x5.75"x4" TALL, 16 LBS EACH, THREE 10" LAG BOLT AND ANCHORS, WHITE OR ADA BLUE



NOTE:
1. MAINTAIN 36" FROM FACE OF WHEEL STOP TO VERTICAL OBSTRUCTIONS OVER 6" CURB HEIGHT

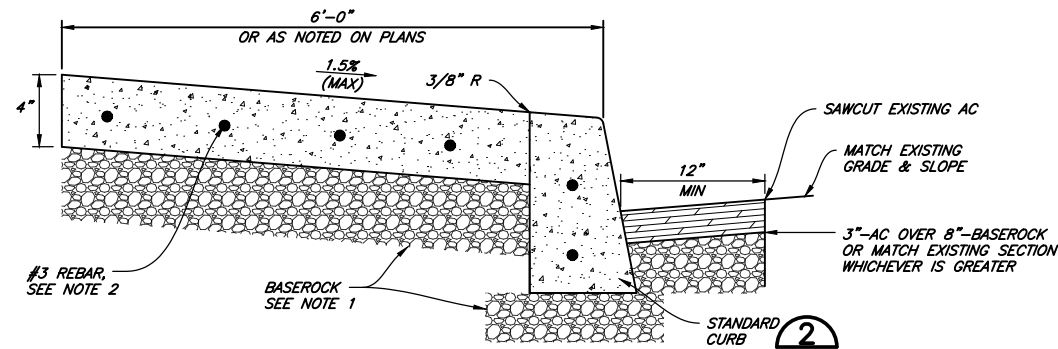
DETAIL 4
NTS
(WHEEL STOP)



DETAIL 5
NTS C-1
(PARKING STALL)

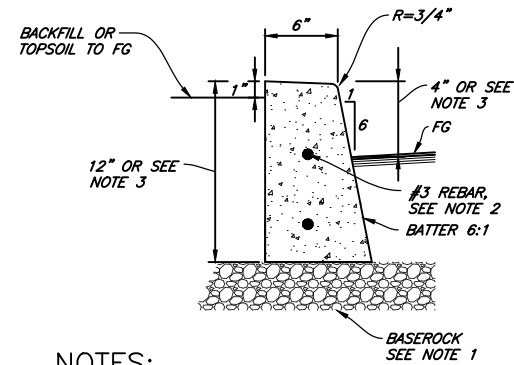


VERIFY SCALES		275 MARKET AVENUE COOS BAY, OR 97420 WWW.SPA-ES.COM 541-266-9880	SPA-ES
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DESIGN	DFG	PORT OF BANDON MARKET PARKING IMPROVEMENT PLAN BANDON, OREGON	ADA DETAILS
DR	DSR		
CHK	RFS		
APVD			
NO.	DATE	REVISION	BY
SHEET		D-3	
SEQ			
DATE		4/2021	
PROJ. NO.		618074	



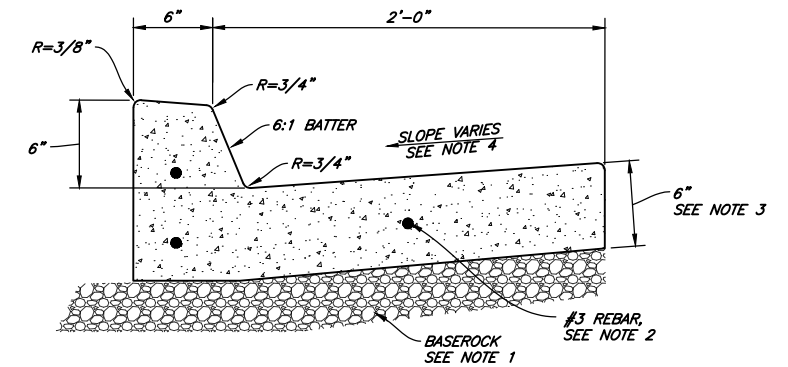
- NOTES:**
1. CLASS B BASEROCK (4" MIN DEPTH UNDER SIDEWALK, 6" MIN DEPTH UNDER CURB, DRIVEWAY APRON AND WINGS.)
 2. WHERE (N) CURB AND SIDEWALK MEETS (E) CURB AND SIDEWALK, INSTALL #3 REBAR DOWELED AND GROUDED (MIN EMBEDDED LENGTH = 3") AT 12" OC, WITH A MINIMUM OF 6" OF REBAR EXPOSED.
 3. MATCH EXISTING DIMENSIONS.

DETAIL 1
NTS
(CURB & SIDEWALK)



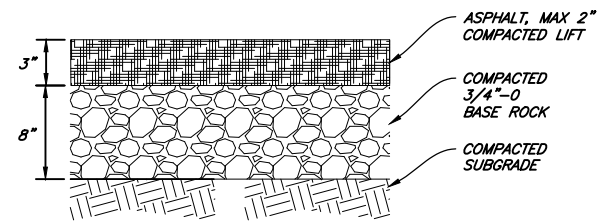
- NOTES:**
1. CLASS B BASEROCK (4" MIN DEPTH UNDER SIDEWALK, 6" MIN DEPTH UNDER CURB, DRIVEWAY APRON AND WINGS.)
 2. WHERE (N) CURB AND SIDEWALK MEETS (E) CURB AND SIDEWALK, INSTALL #3 REBAR DOWELED AND GROUDED (MIN EMBEDDED LENGTH = 3") AT 12" OC, WITH A MINIMUM OF 6" OF REBAR EXPOSED.
 3. MATCH EXISTING DIMENSIONS.

DETAIL 2
NTS
(STANDARD CURB)

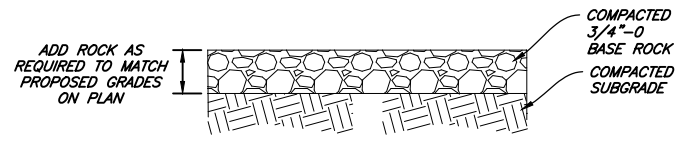


- NOTES:**
1. CLASS B BASEROCK (4" MIN DEPTH UNDER SIDEWALK, 6" MIN DEPTH UNDER CURB, DRIVEWAY APRON AND WINGS.)
 2. WHERE (N) CURB AND SIDEWALK MEETS (E) CURB AND SIDEWALK, INSTALL #3 REBAR DOWELED AND GROUDED (MIN EMBEDDED LENGTH = 3") AT 12" OC, WITH A MINIMUM OF 6" OF REBAR EXPOSED.
 3. MATCH EXISTING DIMENSIONS.
 4. SLOPE 5.0% NORMAL, VARIES AS REQUIRED FOR DRAINAGE, EXCEPT AT CURB RAMPS 4.0% (MAX).

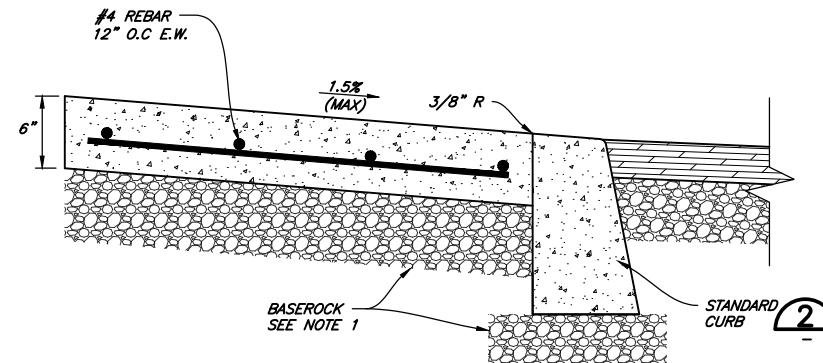
DETAIL 3
NTS
(CURB & GUTTER)



DETAIL 4
NTS
(ASPHALT PAVEMENT SECTION)

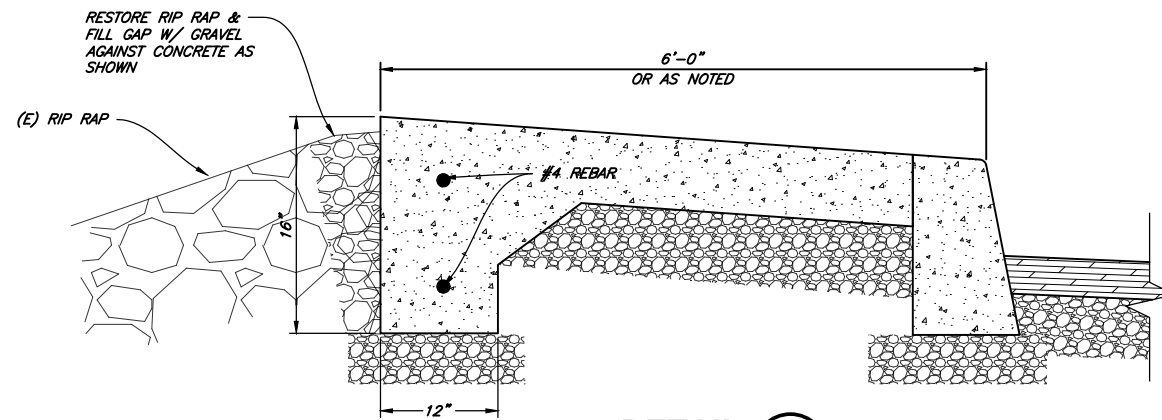


DETAIL 5
NTS
(GRAVEL SURFACE SECTION)

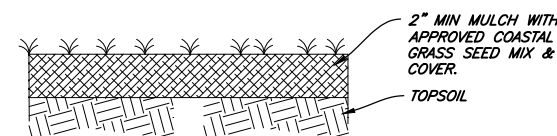


- NOTES:**
1. CLASS B BASEROCK (6" MIN DEPTH UNDER CURB, DRIVEWAY APRON)

DETAIL 6
NTS
(CURB & SIDEWALK AT ROLL UP DOOR)



DETAIL 7
NTS
(THICKENED EDGE SIDEWALK)



DETAIL 8
NTS
(LANDSCAPE SECTION)



SAVED: 4/19/2021 5:54 PM DRABER, PLOTTED: 4/20/2021 5:14 PM GRABER, DAN
 \\CoosBay-ES\Projects\2018\618074-PortofBandon\Draws\618074-DTL5.dwg

VERIFY SCALES
 BAR IS ONE INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, VERIFY SCALES ACCORDINGLY

275 MARKET AVENUE
 COOS BAY, OR 97420
 WWW.SAN-LEON.COM
 541-266-9880



DESIGN	DATE	BY
DPG		
DR		
CHK		
APVD		
NO.	REVISION	

PORT OF BANDON
 MARKET PARKING IMPROVEMENT PLAN
 BANDON, OREGON
CIVIL DETAILS

SHEET
D-4
 SEQ
 DATE 4/2021
 PROJ. NO.
 618074